



# Town Of Davie

TRANSIT ORIENTED CORRIDOR



Section  
12-32.600TRANSIT ORIENTED CORRIDOR (TOC) DISTRICT

Section 12-32, subdivision 600 shall be known as the "Transit Oriented Corridor District."

TOC-N: Transit Oriented Corridor – North

TOC-C: Transit Oriented Corridor – Central

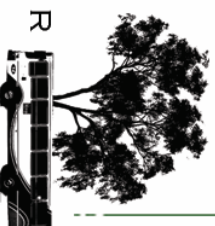
TOC-S: Transit Oriented Corridor – South

Section  
12-32.601GENERAL

The Transit Oriented Corridor Districts are specifically created to implement and encourage the redevelopment of lands designated as Transit Oriented Corridor on the Future Land Use Plan Map of the Town of Davie Comprehensive Plan. The TOC District generally includes right of way and private properties abutting or proximate to the State Road 7/U.S. 441 corridor and comprising approximately 925 as shown in Map 1, TOC Area Map. This area is designated on the Town of Davie (FLUM) Future Land Use Map and the Broward County Land Use Plan Map (BCLUPM) as Transit Oriented Corridor (TOC).

This Section provides general guidance and specific standards necessary to promote the goals, objectives and policies contained within the Future Land Use Element and are designed to maximize the development potential of the State Road 7/U.S. 441 Corridor. Redevelopment within the TOC districts shall focus on building a strong mix of vertical and horizontal commercial, office, research & development land uses compatible with the existing industrial uses and promote shopfronts and commercial uses at street level where appropriate, within a wide pedestrian-friendly and multimodal network. Residential infill shall be compatible with adjacent uses in upper stories of mixed use areas and walkable residential neighborhoods. No residential development shall be permitted in unsuitable areas with unmitigated noise levels.

The Town of Davie Comprehensive Plan specifically calls for the adoption of development regulations to implement the new TOC Future Land Use Category (Resolution 2009-19, Adopted August 19, 2009). The materials in this Section have been shaped by the input of participants in community workshops, public hearings, and technical effort by Town staff and planning and design consultants.

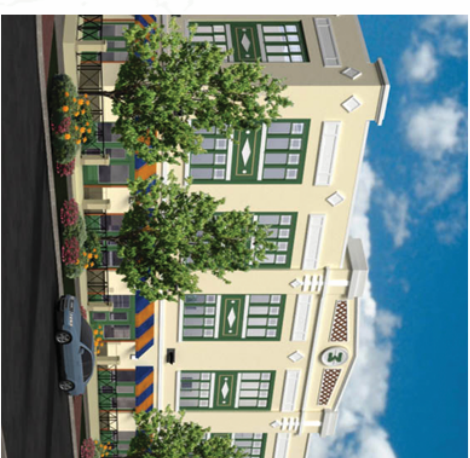




## A EXPANSION OF USES

Prior to the designation of the Transit Oriented Corridor Future Land Use Map category and Transit Oriented Corridor Districts (TOC-N, TOC-C, TOC-S) properties within the Transit Oriented Corridor District were generally designated for commercial and industrial uses.

In general, the Transit Oriented Corridor Districts expand the range of uses that may be permitted for properties within the Transit Oriented Corridor to include a mix of commercial, office, research and development with residential and retail when compatible. A general comparison of building functions and new uses is provided in Section 12-32.606. Uses within the TOC Districts may be permitted without the need or cost for individual comprehensive plan amendments or zoning variances provided they conform to the provisions of this section.



## B EXISTING STRUCTURES & USES

Nothing in this Section shall impair or diminish the rights of the existing land owners or their successors or assigns to continue the use and enjoyment of their properties consistent with the current zoning at the time of adoption of this Section. Existing buildings and their uses shall continue. New development, expansions or changes in use shall be subject to the provisions of the Transit Oriented Corridor Districts excluding the protection of existing uses. Property owners considering new development, expansions or changes in use are encouraged to work with the Town of Davie and the Town of Davie Redevelopment Agency prior to submitting an application for development.



The expansion of a building shall comply with these regulations if the cumulative square-footage of expansion occurring within any five (5) year period will exceed the floor area of a building, as it existed upon the effective date of these regulations, by twenty-five (25) percent or more. In the event that the cumulative five (5) year threshold is met, compliance with these regulations can be achieved without compromising the intended use of expansion and logistics of the building and site layout



## C MASTER PLAN FOR TOC DISTRICTS

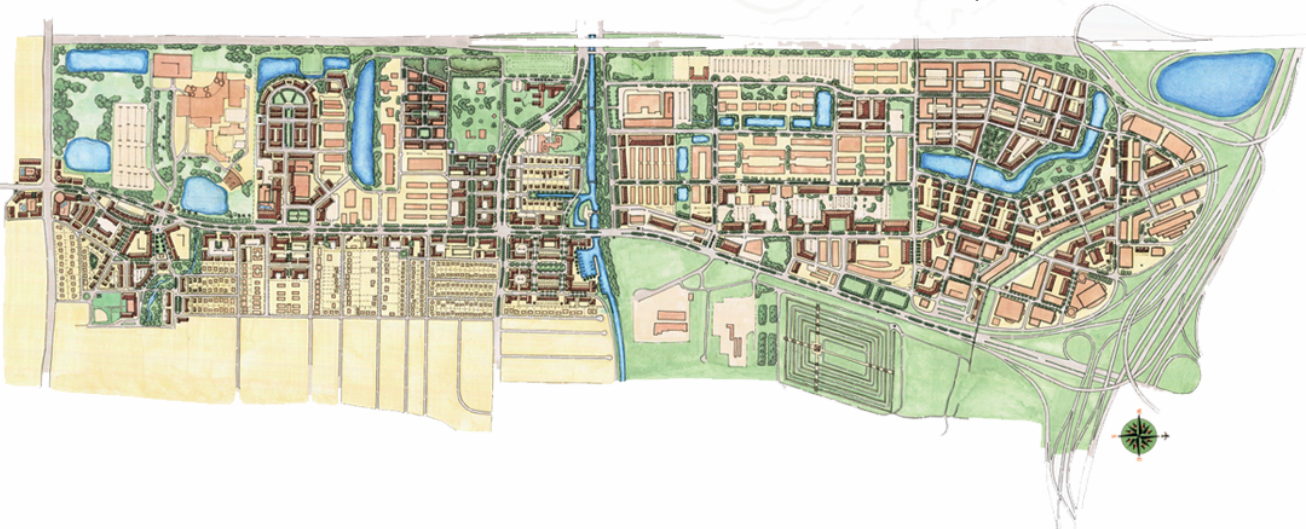
The TOC Future Land Use Map change and the specific TOC Districts are the results of a community-based vision plan. The vision plan was created as part of The State Road 7/U.S. 441 Collaborative community charrette series conducted in 2004. The Town of Davie State Road 7 Charrette Master Plan (Master Plan) was approved by the Town Council on September 7, 2005 by Resolution #2005-236.

The community vision plan presents the ideals and actions that provide the framework to transform the corridor from a single-use auto-oriented urbanism to a pedestrian and transit-oriented mixed-use development pattern. By redesigning the current mix of single-use industrial areas, and commercial parcels with a mix of building types, uses, and public and private frontages, the State Road 7/U.S. 441 corridor could create a tremendous opportunity for economic investment. While change in the overall character of the corridor is the long-term objective of this plan, not all properties in the TOC District will change. The Master Plan illustrates how this incremental change can be redeveloped over a period of time.

The Master Plan serves as a blueprint for creating vibrant mixed-use districts that capitalize on long-term mobility transit strategies for the State Road 7/U.S. 441 corridor. The Town of Davie Comprehensive Plan designates the areas of the TOC District for mixed-use development necessary to accommodate significant economic development and a range of housing opportunities.

To achieve a predictable environment, as detailed in the Master Plan, a new and supportive future land use and development code changes were necessary to permit and encourage significant amounts of new residential development and mixed-uses. The first step was the creation of the Transit Oriented Corridor (TOC) Future Land Use Map category and designating the study area of the Master Plan as TOC, consistent with The Broward County Land Use Plan. Specific objectives and policies can be found within Future Land Use Element of the Town of Davie Comprehensive Plan.

These regulations provide the opportunity to create a Community Development District (CDD) or Special Assessment District (SAD) within the TOC to facilitate the goals outlined in the Master Plan and specified in this section.

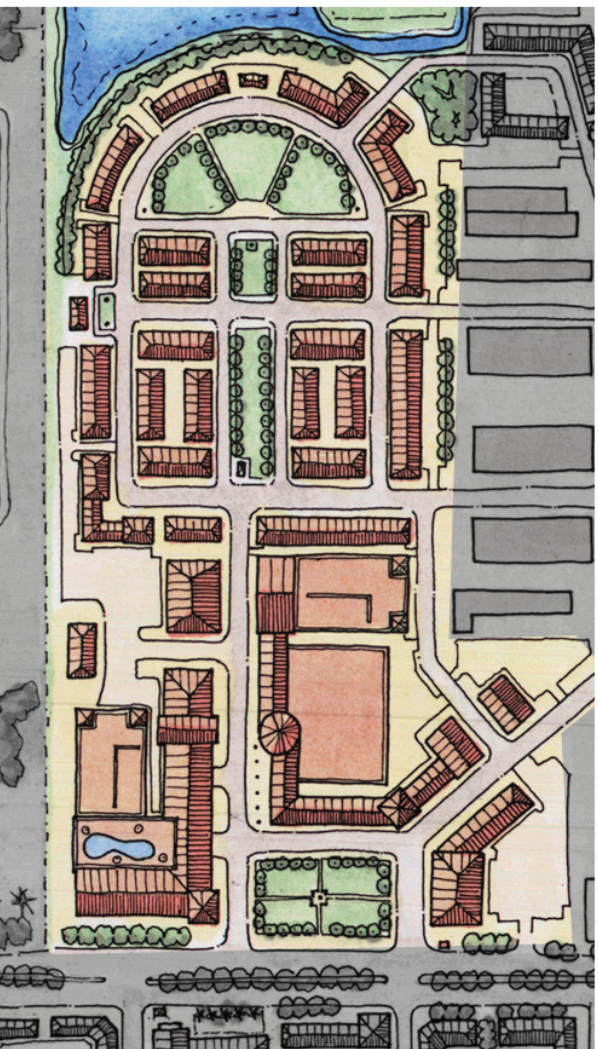




## C MASTER PLAN FOR TOC DISTRICTS (continued)

This Section (12-32.600), Transit Oriented Corridor (TOC) District, of the Town's development code explains and illustrates to developers, review staff, residents and elected officials as to the types of streets, buildings, heights, and mixes of use that create the desired "form" presented in the Master Plan. Form-based codes are particularly suited for this purpose and may provide for, among other subjects, the establishment of building design, site design, and other standards that apply to all development proposed to be constructed or carried out in areas of the Town subject to the form-based code. This form-based code is transect-based with a corresponding Regulating Plan that prescribes the appropriate land uses, and project and site design principles relating to appropriate building types, housing mix, integration of land uses, and appropriate form for the pedestrian orientation, streetscape, and public realm in the subject area.

The development regulations contained in this Section are intended to provide clarity and flexibility in the rules for mixed-use development and to promote and implement redevelopment of the TOC District consistent with the objectives of the Master Plan and Town of Davie Comprehensive Plan. The provisions outlined for the TOC allow for development to be permitted without the traditional Council approval process.



*Town Center potential mixed use center adjacent to the Seminole Hard Rock Casino*





## INTENT & PURPOSE

The intent of this Section is to produce a pedestrian oriented form that fulfills the goals, objectives and policies of the Town of Davie Comprehensive Plan. The redevelopment of the TOC District will be achieved over many years as a result of actions by private and public organizations perhaps over a decade or more on some sites. A lengthy build-out period will allow development to respond to the presence of current uses with long term leases, to allow for the real estate market to mature to a point that supports compact development and structured parking, and to benefit from incremental change that will make the corridor a more varied and interesting place. The Town shall act as the steward of this process by maintaining a commitment to the Master Plan's vision, and by approving individual development projects only if they contribute to ultimate achievement of the corridor vision by:

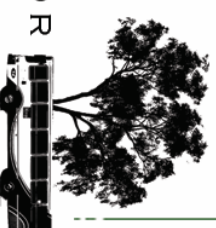
1. Promoting the community vision for an appropriate urban form for future development and redevelopment;
2. Reducing impacts on adjacent uses through building placement and sensitivity of site design and sustainable building standards;
3. Encouraging infill and redevelopment with an emphasis on pedestrian orientation, increased mobility and integration of transit and bicycle systems that provide alternatives to the automobile and promote a sustainable environment;
4. Increasing the relationship between primary building entrances, the public sidewalk, and the public with entrances to building shopfronts, workplace and housing oriented directly to the public street;
5. Providing opportunities for a range of housing types and price levels to accommodate diverse ages and incomes;
6. Planning transportation corridors in coordination with land use and coordinating the intensity of development proximate to mass transit and encouraging a sustainable environment;
7. Providing that TOC development be compact, pedestrian-oriented and mixed-use;



## INTENT & PURPOSE (continued)

8. Providing that ordinary activities of daily living occur within walking distance of most dwellings, allowing independence to those who do not drive;
9. Providing for interconnected networks of tree-lined or shaded roads to be designed and created incrementally to improve pedestrian access to transit and to disperse and reduce the length of automobile trips;
10. Creating a sustainable district by using proven green technologies and LEED standards;
11. Promoting the small scale of newly-formed blocks with public open spaces to serve corridor residents, workers and visitors for increasing social gathering, active and passive recreation, and visual amenities;
12. Promoting appropriate building densities and land uses be provided within walking distance of transit stops;
13. Creating a range of open space including parks, squares, and playgrounds and positioning buildings to front streets and open spaces;
14. Requiring buildings, streetscapes and landscaping to contribute to creating community living spaces;
15. Designing the public realm and site plans that emphasize comfort, safety, and distinctive identity, with the provision of urban greenways along major corridor roadways containing a high level of transparency at the side-walk level;
16. Maximizing the development potential for the TOC District;
17. Providing a form based code as a guide for change to promote a harmonious and orderly evolution of the TOC District redevelopment area; and

The Conceptual Master Plan depicts the vision and may be used to better interpret this Section. Where there is conflict between the Conceptual Master Plan and the text of this section, the text shall govern.





## APPLICABILITY

After the effective date of this Section, the building design and site design standards and any and all other land use and development requirements, standards, regulations, or other provisions set forth in this Section shall apply to all new development and redevelopment zoned TOC, notwithstanding any other provision to the contrary, including but not limited to, standards regarding setbacks, offstreet parking requirements, and maximum lot coverage, building height, or yard area. These standards and guidelines shall be used to evaluate private development projects or improvement plans. In any instance where the requirements, standards, regulations, or other provisions set forth in an adopted development code conflict with the requirements, standards, or regulations other set forth in other provisions, the standards in this Section shall take precedence.

The TOC District established in the Town of Davie's Comprehensive Plan provides sufficient intensity of development to create mixed use districts and building types that accommodate the offices, commercial, retail, lodging and dwellings, including row-houses and apartments that support this research and development industrial area. The TOC District is a tight network of streets and blocks with wide sidewalks, steady street tree planting and buildings set close to the frontages. Compact urban development is key to successful place-making that shall differentiate the TOC District from other suburban arterials and industrial areas. Compact urban development means efficient use of land characterized by multi-story buildings with most parking provided in the rear of the parcel or in structures that follow urban-oriented design principles.

The transformation of the State Road 7/U.S. 441 corridor requires a range of changes to the public right of way. The result shall be the creation of Districts where the street, landscape, and buildings work together as a unit to create a distinctive and memorable urban place. Central to this vision is the creation of mixed-use and multi-story buildings along the State Road 7/U.S. 441 corridor and a departure from the present standard of single-story retail buildings with surface parking. The mixed uses along the State Road 7/U.S. 441 corridor shall not be established through the introduction of mixed use buildings exclusively. Office buildings or properly-scaled row-houses and apartment buildings may be part of the mixed use District provided that each is located within easy walking distance of shops and services with the creation of a greatly-enhanced pedestrian realm which shall support investment that attracts residents and businesses to the corridor.

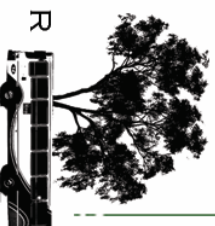
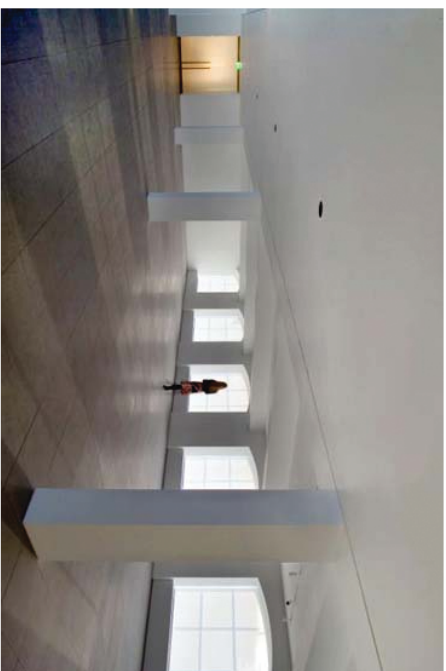




APPLICABILITY (continued)

Buildings in the TOC District shall vary in height with ground-floor retail, restaurants, and other active uses when possible. Upper-story housing and office uses will provide patrons to these businesses. For people who live or work in the corridor area (or for those who both live and work), the location shall offer the benefits of convenience and amenity in a location that is newly walkable, central to Town of Davie, and linked to the larger region via the Broward County Transit Breeze and Davie's Transit Network.

A wide range of uses are welcome in the corridor so that its role as a convenient center of research and development and commerce continues and expands. Large footprint buildings shall incorporate walkable features through the integration of liner shops or pedestrian oriented buffers in order to avoid the monotony of blank walls that now characterize too many retail, industrial and office formats. The cumulative effect of these characteristics is more than improvement in the visual environment. Convenience, accessibility and identity shall all be enhanced, leading to a "sense-of-place" which has been described as an intrinsic value that in turn results in increasing rents, retail sales, leasing demand, and capital value. It is also a place where future development will bring significant transformation, as the community vision set forth in the Town of Davie's Comprehensive Plan, is realized during the coming years and decades.



HOW TO USE THIS CODE

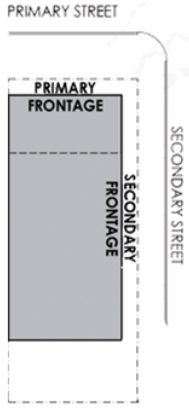
To find the development standards that apply to a particular parcel of land, the following steps should be taken:  
(See graphic instructions on inside rear cover)

1. Locate the property in question on the Regulating Plan Map.
2. Note the TOC District(s) in which the property is located (TOC-N, TOC-C, or TOC-S)
3. Refer to Uses Permitted Table for the TOC District.
4. Locate the property in question on the Height Regulating Plan.
5. Note the applicable height or building character limitations.
6. Locate the property's primary frontage, "Primary-Secondary Frontage" Definition.
7. Note the applicable frontage and right-of-way requirements for roadways in the Recommended Connections Plan
8. Refer to the "Regulations Summary Sheets" Illustrations

The Zone standards specify the setback and height regulations for buildings, parking and associated site improvements, and also identify the Building Types and Frontage Types that are allowed. A range of Building Types are presented in Building Frontages Illustration. One of the Building Frontages allowed in the applicable Zone must be selected. Note that Building Uses may be combined, as described in Section 12-32.606 (Mixing of Uses). To promote a diversity of Building Uses, and to control the visual scale of new development projects, type mixing is recommended on parcels over thirty-thousand (30,000) square feet in area.

Contact the Town of Davie Planning & Zoning Department or Community Redevelopment Agency to verify information or to learn about potential development incentives.

For properties with primary frontages along the State Road 7 Corridor detailed conceptual site plans or "Tear Sheets" based on the Charrette Master Plan are available for reference. These drawings are also available in Auto-Cad format and may provide guidance on how to apply the code, as applicable.



Regulations Summary

TOC District: (See Map Page 12-32.604)

Table with columns: Building Type, Frontage Type, Height, Setback, etc.

TRANSIT ORIENTED CORRIDOR

Table with columns: TOC District, Building Type, Frontage Type, Height, Setback, etc.



**USES PERMITTED**

No building or structure, or part thereof, shall be erected, altered or used, or land or water used, in whole or in part for other than uses specified in this section and those specified in Article II, Division I of the Town of Davie Zoning Code. General building functions and uses are depicted in Section 12-32.606 B Table of Uses.

**A MIXING OF USES**

Mixing of Uses. Uses can be mixed horizontally or vertically, subject to the rules of this Subsection.

1. Horizontal mixing via separate buildings is permitted, provided that that residential uses are not facing non-residential loading areas.
2. Mixing of residential and nonresidential uses within the same building is encouraged and permitted subject to functionally appropriate separation of the uses, including but not limited to: separate stories; separate access; separation and buffering of residential units from loading areas and noisy nonresidential uses via one (1) or more intervening stories of office use, thicker concrete floors, soundproofing on ceilings, walls and sound-containing openings, operational standards and time limits, or other proven technique acceptable to the Town.





B  
TABLE OF USES

LAND USE TYPE			NORTH	CENTRAL	SOUTH
<b>RESIDENTIAL</b>					
Single Family Detached			N	N	N
Multiple-Family			P	P(1)	P
Live-Work			P	P	P
<b>LODGING</b>					
RV Parks/Campgrounds			N	N	N
Hotel			P	P	P
<b>GENERAL RETAIL/SALES/SERVICE</b>					
Vehicle Related			P(2)	P	P(2)
Commercial Entertainment			P	P	P
Adult Entertainment			P(3)	P(3)	P(3)
Food Service Establishment			P	P	P
Alcohol Service Establishment			P(4)	P(4)	P(4)
Open Air Retail			P(5)	P(5)	P(5)
Service/Care Facilities			P	P	P
Animal Hospital & Animal Day Care			P(2)	P	P(2)
Repair shop			P(2)	P	P(2)
<b>OFFICE</b>					
All			P	P	P
<b>UTILITY</b>					
All			P	P	P
<b>CIVIC/INSTITUTIONAL/EDUCATIONAL</b>					
All			P	P	P
<b>INDUSTRIAL</b>					
Extensive Impact Industry			N	P	N
Limited Impact Industry			P	P	P

P= Permitted Use  
N= Not Permitted

P(#)= Permitted subject to corresponding

Table note:

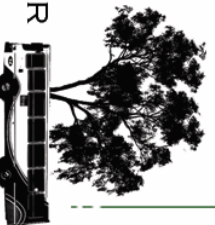
1. New residential uses proposed within areas of existing industrial or other existing potentially incompatible uses shall mitigate negative impacts on site.

2. Uses with potentially extensive impacts, as defined by "Extensive Impact Industry" shall be regulated by the "Extensive Impact Industry" Use Category.

3. See Section 12-34 "Detailed Use Regulations" for conditions of use

4. Establishments serving alcohol within the TOC shall not subject to distance restrictions.

5. Open Air Retail shall be limited to an accessory use and shall not occupy an area greater than twenty percent (20%) of the lot area.



**C** ACCESSORY USES AND OUTBUILDINGS

Accessory Uses and Outbuildings are permitted subject to the provisions of this Section. Accessory dwellings may be part of the principal building, or an accessory building, on any floor, except as specified for ground floor retail uses.

**Section 12-32.606** USES PROHIBITED

Any use not specifically listed in the Table of Uses in Section 12-32.606 is prohibited. Residential uses shall be prohibited in areas within the 60 DNL (Day-Night Average Noise Level) area and subject to noise mitigation requirements between 60 DNL and 65 DNL as specified in Section 12-32.620. The Town administrator holds the authority to evaluate acceptable uses beyond those specified for the TOC district.

**Section 12-32.607** LIMITATION ON USES

Any permitted use may be limited in order to comply with the regulations of this section. Required setbacks, screening and buffers shall be utilized to mitigate negative impacts of existing and proposed uses throughout the TOC Districts. Singularly residential use buildings shall only be permitted within 1000 feet of supporting commercial and office uses. Maximum residential density per unified development shall be 32 units per gross acre. A total of 15% of the residential units within the TOC shall be provided as affordable housing and undiscernable within the community. All residential units, including affordable, shall comply with the regulations of this section.

**Section 12-32.608** PLOT SIZE

The minimum plot size, excluding public rights-of-way, shall be ten thousand (10,000) square feet with a minimum of one hundred (100) feet of street frontage.

**Section 12-32.609** REGULATING PLAN

The Development Regulations in this document are applied to properties within the Transit Oriented Corridor (TOC) Plan Area as indicated on the Regulating Plan Map. The development Standards and Guidelines for all TOC District Zones are contained in Section 12-32.600. Development Standards and Guidelines pertain to Building Placement, Height, Frontage Types, Building Types, Use, Parking, Streets and Blocks, and Landscaping, Signage and Open Space.



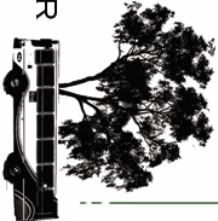


**A** REGULATING PLAN OVERVIEW

To ensure that redevelopment activities are consistent with the intent of the Comprehensive Plan and the Conceptual Master Plan is realized, a series of TOC Districts are established as the basic organizing principle for the regulations contained in this Section (see Regulating Plan).

A TOC District is an area of cohesive physical character. Districts are organized according to criteria that influence their future urban character. In addition to TOC Districts, regulations controlling building placement, building height, frontage and building types, use, parking and open space standards are organized by the Height Regulating Plan and the Recommended Connections Plan. The Town of Davie TOC District and Regulating Plan are organized into 3 distinct Districts:

- Transit Oriented Corridor - North (TOC-N)
- Transit Oriented Corridor – Central (TOC-C)
- Transit Oriented Corridor – South (TOC-S)





### **B** TOC-N TRANSIT ORIENTED CORRIDOR - NORTH

Parcels Designated as TOC-N represent a combination of existing industrial uses with an emphasis on a higher utilization of the site's location and proximity to major transportation and educational facilities within Broward County. As the mixed use research and development employment area emerges in this district, compatible office space and residential uses will be focused within this area and conform to the built form regulations of this section. An active primary frontage edge will emerge through vertical mixed use as well as residential and workplace buildings.

### **C** TOC-C TRANSIT ORIENTED CORRIDOR - CENTRAL

Parcels designated as TOC-C typically contain significant existing industrial uses and the integration of compatible office and residential uses will be dependent on the site and the capacity to provide the appropriate mitigation measures. Streetfronting uses are required and will be supported by a redesign of the streetscape to create a highly walkable zone that will connect within the TOC Districts and provide access to multi-modal facilities.



### **D** TOC-S TRANSIT ORIENTED CORRIDOR - SOUTH

Parcels designated as TOC-S include a focus on compatible office space and residential uses and the integration of a Town Center intended to create a unified development plan for a high-intensity, highly walkable mixed use district with the following features:

1. A Main Street with ground floor shops and restaurants that connects a Town Square with State Road 7
2. Shopfront buildings assure frequent entries and a high level of transparency into stores and restaurants.
3. Urban fabric with housing and/or workplace uses in buildings ranging in height.
4. Highly amenitized public realm including a Town Square, ornamental street trees, lights and public art.
5. The parking is consolidated in the rear of properties and at curbs of public streets.



DESIGN STANDARDS

Building placement, setbacks, heights, frontage types, building types, use, parking, new streets, blocks, landscaping, signage and open spaces are presented in Section 12-32-610 through Section 12..32-615.

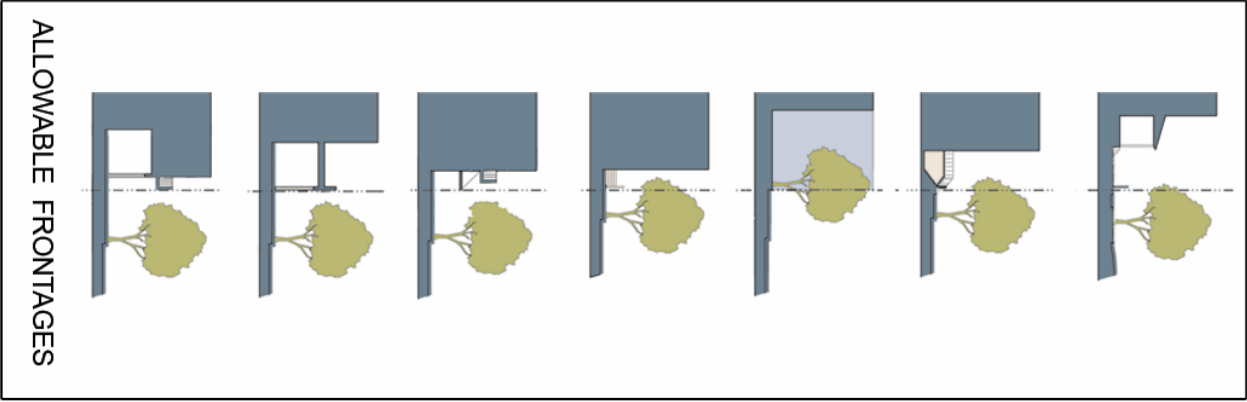
**A BUILDING PLACEMENT & SETBACK**

The Building Placement regulations contained in this section assume the implementation of a two hundred (200') minimum Trafficways reservation width for parcels fronting the State Road 7 corridor.

**B PRIMARY AND SECONDARY FRONTAGE SETBACK**

Placement of buildings will need to reflect either (1) location of back of sidewalk following street reconstruction, or (2) if street reconstruction has not yet taken place, approved Town plans for sidewalk reconstruction that re-locate the back of sidewalk. Front setback is defined as the permitted distance from the back-of-sidewalk line along a Primary Street to the primary building façade as shown in Primary Frontage Definition. The Allowable Frontages Diagram outlines the frontage types required within all TOC districts. All Frontage Setbacks shall maintain minimums as required by section 12-32.619. The Primary Frontage Setback shall not exceed the maximum setback necessary to satisfy any required Trafficways reservation, and will be the required Build-To-Line. Public sidewalks constructed along the Frontage shall be dedicated to the Right-of-Way in order to meet all minimums required. See definitions for Build-To-Line and Primary Frontage.

At required setback areas, awnings, entrance porticos, porches, stoops, stairs, and entrance overhangs, the Town may allow to encroach within the required front setback as shown in the frontage type illustrations. All encroachments must have legal agreements acceptable to the Town Attorney. Balconies, bay windows, eaves, and other permitted encroachments may extend up to a maximum of eight (8) feet into the private frontage. At zero-setback areas, building overhangs such as trellises, canopies and awnings may extend horizontally into the public frontage a minimum of eight (8) feet and extend to a distance not to exceed 2' from the curb. These overhangs must provide a minimum of eight (8) feet clear height above sidewalk grade. No encroachments are permitted into the Trafficway reservation areas. No architectural support of encroachments shall be constructed within the Clear Zones identified for sidewalks or impede the flow of pedestrian or bicycle traffic.





**C REAR SETBACK**

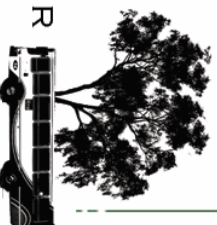
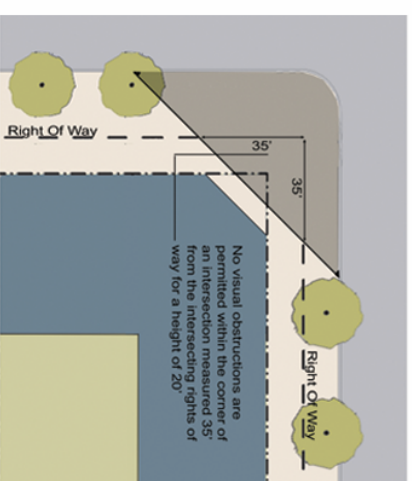
Rear setback is defined as the required minimum distance from the rear property line to the primary building. Rear Setbacks must be maintained to accommodate rear alleyway creation where designated on the Recommended Connections Plan. The minimum rear setback shall be five (5) feet for properties with an alleyway or rear road serving the same function. All other properties within the TOC districts shall maintain a minimum fifteen (15) foot setback.

**D BUILDING FRONTAGE**

Structures or screening shall meet the Build-to-Line for as much of the property frontage as feasible, accommodating for access and sight line requirements. For uses without a Primary Structure at the property frontage, the required wall or screening shall meet the frontage coverage requirements for the applicable TOC District. Frontage coverage is defined as the minimum percentage of the length of the frontage coverage zone that shall be occupied by the front façade(s) of the primary building(s), screening or wall. In the TOC-N and TOC-S Districts, the minimum frontage buildout shall be seventy (70) percent. In the TOC-C District, the minimum frontage buildout shall be sixty (60) percent. In order to connect the public sidewalk with courtyards and parking lots in the interior or at the rear of a parcel, development may incorporate a passage that counts towards the frontage coverage requirements. A passage is a paved pedestrian walkway penetrating the building to access interior parking, courtyards, or other public spaces. The width of a passage may not exceed 15 feet.

**E SIGHT AT CORNERS**

Structures shall be built to the corners of primary roadways. Buildings, screening structures and walls must "hold the corner" of the parcel at the intersection of two primary streets. The build-to-corner location is defined by the required front and side setback lines. Where the Build-To-Corner Building Placement is required, new development must meet this requirement by siting the building at its street corner. No visual obstructions are permitted within the corner of an intersection measured thirty-five (35) feet from the intersecting rights-of-way for a height of twenty (20) feet.



**G** HEIGHT REGULATING PLAN

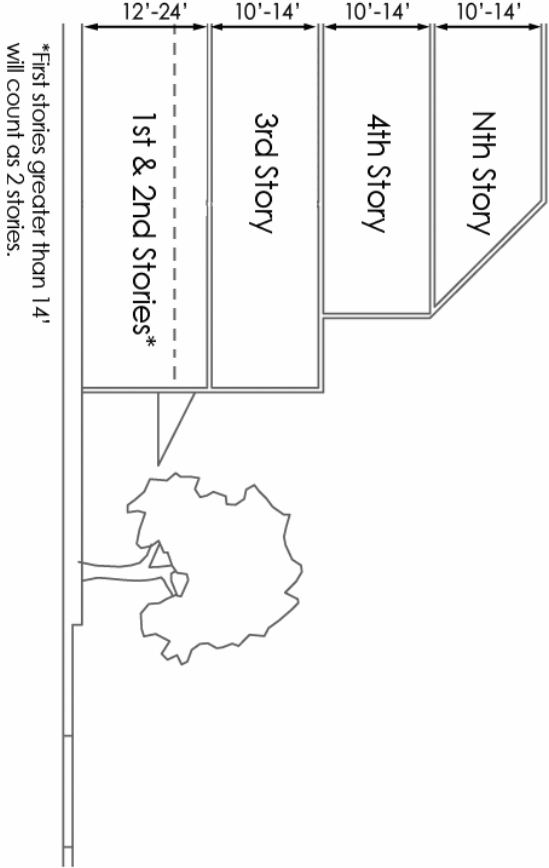




F

HEIGHT LIMITS

No building or structure shall be erected or altered to exceed the heights identified in the Height Regulation Plan. No habitable level or story with in a building shall be more than 14 feet in height from finished floor to finished ceiling, except that the first floor may be up to twenty-four (24) feet in height. Rooftop antennae and elevator shafts are not counted in these height limitations. Rooftop mechanical equipment must be placed in enclosures or screened.



**PARKING STANDARDS**

This section contains development standards and design guidelines to ensure that parking throughout the Plan Area is convenient and accessible, accommodates all land uses, and supports the Plan's intended goals, including:

1. Enable people to park once at a convenient location and to access a variety of commercial enterprises in pedestrian friendly environments by encouraging Shared Parking.
2. Avoid adverse parking impacts on neighborhoods adjacent to redevelopment areas.
3. Maximize on-street parking.
4. Encourage parking to be located in the Third Layer of the lot.
5. Provide flexibility for redevelopment of small sites.

Parking shall be connected with the street by a driveway as stated under Access in Section 12-32.611 C. New on-street parking spaces provided adjoining ground floor residential uses may be counted toward the minimum parking requirement for that property. Parking types are defined as either surface or structured and shall be limited to areas and lot layers depicted in the Summary Sheets.

**A**

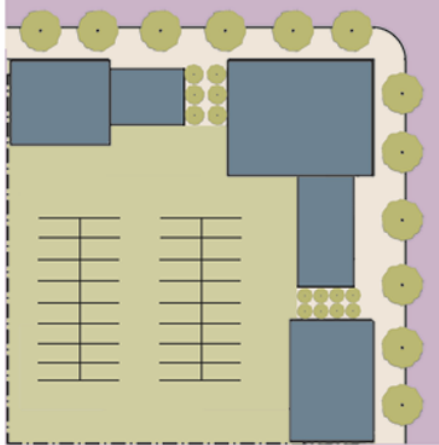
**SURFACE PARKING**

**Open Surface Parking**

A surface parking lot that fully or partially extends to a parcel's street frontage(s) (including landscaped setback areas) and is not sited behind a building(s) located along the parcel's primary street frontage(s). Open parking lots in the First Layer of the lot are prohibited. Open parking in the second lot layer is discouraged and limited to no more than twenty (20) percent of the front lot width. All parking areas are encouraged in the third lot layer. Open parking areas shall be masked from the Frontage by a Building or Streetscreen.

**Wrapped Surface Parking**

A surface parking lot where a building(s) is located between the parcel's street frontage(s) and the parking lot. Except for driveway access, the parking lot is sited behind buildings and no portion of the parking lot is visible from the primary street.





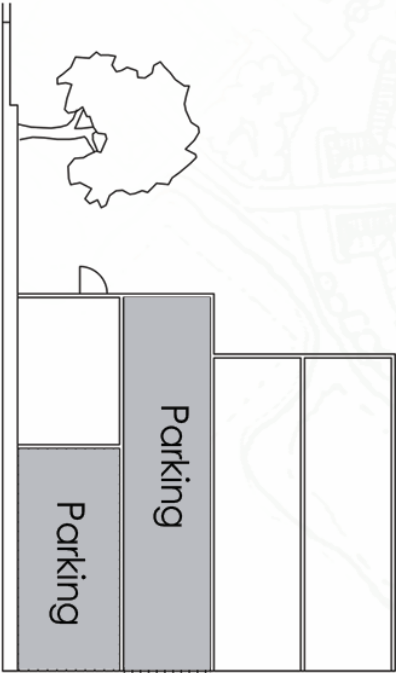
**B** STRUCTURED PARKING

**Exposed**

An above-ground parking structure that is fully or partially exposed to the primary front street(s) on the ground level. Exposed parking structures shall not be located in the first lot layer on the ground level. The parking structure may be exposed to the building's street frontage(s) and second and or third lot layers on upper levels.

**Wrapped on Ground Level**

An above-ground parking structure where non-parking uses are integrated into the ground level of the building along the parcel's primary front street frontage(s). Non-parking uses are encouraged to be integrated into the building along secondary street frontages, further hiding the parking structure from view. The parking structure may be exposed to the building's street frontage(s) on upper levels.



**Wrapped on All Levels**

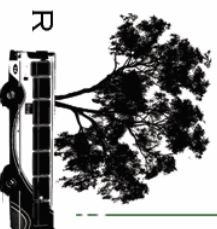
An above-ground parking structure where non-parking uses are integrated into the building along the parcel's front street frontage(s) on all levels of the building. Nonparking uses are encouraged to be integrated into the building along all street frontages, further hiding the parking structure from view.



**C** PARKING CALCULATIONS (continued)

5. Cross-access and parking agreements will be required where appropriate to facilitate smooth traffic movement from one property to another.
6. It is the responsibility of the applicant to document the availability of proposed off-site parking locations in order to meet the minimum number of required parking spaces.
7. Shared parking factors are available for separate functions within the same building, lot or block. In the case of mixed uses, the total requirement of off-street parking spaces shall be the highest of the requirement of the various uses computed for the following five (5) separate time periods: Weekdays (daytime); Weekdays (evening); Weekends (daytime); Weekends (evening); Nighttime. For purposes of calculating the requirement of the various uses for the various separate time periods, the percent of parking required shall be according to the Shared Parking Table:

	Weekday		Weekend		Nighttime
	Daytime (6 a.m.--5 p.m.) (Percent)	Evening (5 p.m.--midnight) (Percent)	Daytime (6 a.m.--5 p.m.) (Percent)	Evening (5 p.m.--midnight) (Percent)	Midnight (Midnight--6 a.m.) (Percent)
Office	100	10	10	5	5
Retail	75	75	100	70	5
Lodging	75	100	75	100	75
Restaurant	50	100	100	100	10
Entertainment	40	100	90	100	10
Residential	60	100	100	100	100





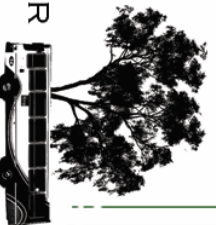
## C PARKING CALCULATIONS

1. Buildable density on a Lot shall be determined by the sum of the actual parking calculated as that provided (1) within the Lot (2) along the parking lane corresponding to the Lot Frontage, and (3) by agreement from a shared parking facility within a standard pedestrian shed of a quarter ( $\frac{1}{4}$ ) mile or thirteen-hundred and twenty (1,320) feet of the development site, if available. Lots immediately adjacent to a designated Broward County Transit stop or Town of Davie Circulator stop may count the stop for up to five (5) required parking spaces, a maximum of ten (10) spaces if the bus stop has a shelter from sun and rain, and a maximum of fifteen (15) spaces if the bus stop has a shelter from sun and rain and has an operable variable schedule "real-time" display or automated ticket dispenser, provided there is direct access to the bus stop via a minimum five (5) foot sidewalk.
2. Liner Buildings less than thirty (30) feet deep and no more than two stories shall be exempt from minimum parking requirements.
3. Sites under twenty-thousand (20,000) square feet in land area, industrial uses and warehouses have no minimum parking requirements.
4. Sites over twenty-thousand (20,000) square feet in land area have the following shared-parking requirements and parking may be provided off-site within a distance of thirteen-hundred twenty (1,320) feet:
  - Office** : 1 space per 300 square feet of gross floor area
  - Retail** : 1 space per 250 square feet of customer service area
  - Lodging** : 1 space per every 2 lodging units and a minimum of 1 additional space per every 5
  - Restaurant** : 1 space per 250 square feet of customer service area
  - Entertainment/Civic/Assembly** : 1 space per every 5 seats with a minimum of 1 space per 1000 square feet
  - Residential** : 1.5 spaces per principal dwelling unit and a minimum of one additional space per every 10



## D BICYCLE PARKING

1. For residential development, the developer shall provide one (1) tenant bicycle parking rack or bicycle locker (2-bike capacity) per 3 units and 1 visitor bicycle parking rack (2-bike capacity) per 50 units.
2. For retail development, the developer shall provide one (1) employee bicycle parking rack or bicycle locker (2-bike capacity) per five-thousand (5,000) square feet of gross floor area and one (1) visitor/customer bicycle parking rack (2-bike capacity) per twelve-thousand five-hundred (12,500) square feet of gross floor area.
3. For office development, the developer shall provide one (1) employee bicycle parking rack or bicycle locker (2-bike capacity) per seven-thousand five-hundred (7,500) square feet of gross floor area and one (1) visitor/customer bicycle parking rack (2-bike capacity) per twenty-thousand (20,000) square feet of gross floor area.
4. All bicycle parking facilities shall be highly visible to intended users. On average, one bicycle rack should be located approximately every four-hundred feet. Bicycle racks provided at designated bus stops shall not count towards meeting the bicycle parking requirements. The bicycle parking facilities, including the bicycle, shall not encroach on any area in the public right of way intended for use by pedestrians.





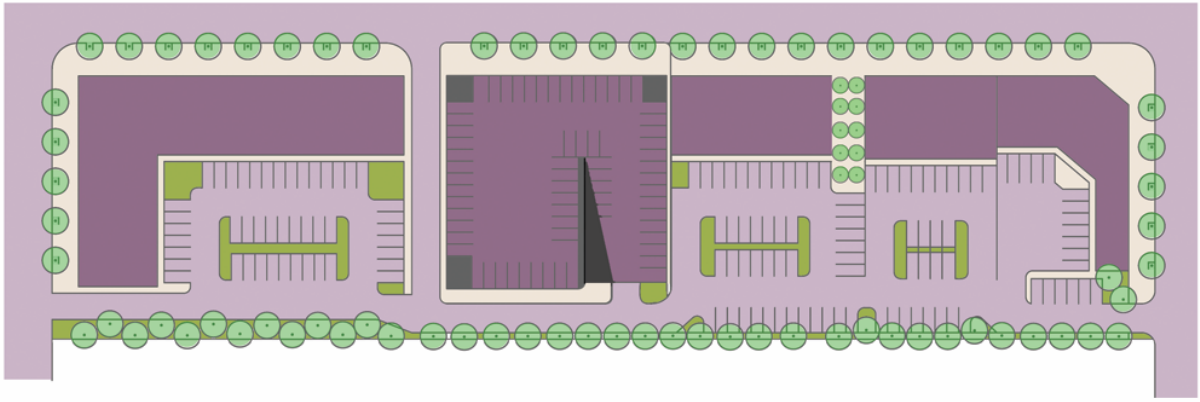
ACCESS

Vehicular access to parking facilities shall be provided from alleyways, rear roads, or driveways wherever possible from side streets, rear streets or internal lot streets in order to maintain a pedestrian oriented environment and promote multi-modal connections.

A CURB CUTS

In order to provide the maximum safety with the least interference to the traffic flow on public streets, and to provide ease and convenience in ingress and egress to private property, the number and location of driveways shall be regulated relative to the intensity or size of the property served and the amount of frontage which that property has on a given street as follows: Along all streets, the maximum number of curb cuts associated with a single building is one (1) two-lane curb cut or two (2) one-lane curb cuts. Parking facilities on adjoining lots are encouraged to share access points and driveways subject to a recorded covenant running with the affected uses.

1. The maximum width of curb cuts is twelve (12) feet for a one-lane and twenty-four (24) feet for a two-lane driveway. The minimum width of rear alleyways, rear road, or driveway is eleven (11) feet for a one-lane and twenty-two (22) feet for a two-lane alleyway, rear road, or driveway.
2. Driveway entrances shall be setback a minimum of five (5) feet from adjoining properties, and a minimum of six (6) feet from adjacent buildings. Further, centerlines of rear alleyways, rear roads, or driveways shall be setback a minimum of sixteen (16) feet and a maximum of thirty-five (35) feet from adjoining rear properties, and a minimum of seventeen (17) feet from adjacent buildings.
3. The total width of parking access openings on the ground level of structured parking shall not exceed 30 feet.
4. Three (3) driveways entering on a particular street from a single property or development may be permitted if all other requirements of this Section are met and if the minimum driveway spacing between adjacent driveways equals or exceeds one hundred (100) feet.



## B DRIVEWAYS

In general, not more than three (3) driveways will be permitted from a single property or development. However, in the case of extensive property development (property exceeding ten (10) acres in total land area and/or containing more than one thousand (1,000) parking stalls), additional driveways may be permitted provided all other requirements of this Section are met and the minimum driveway spacing between adjacent driveways equals or exceeds three hundred (300) feet. The minimum driveway spacing between driveways on adjacent properties shall be fifty (50) feet. This driveway spacing may be modified by the Broward County Traffic Engineering Division if a traffic engineering study acceptable to the Division demonstrates that public safety will not be adversely affected by such modification.

## Section 12-32.613 STREETS & BLOCKS

New streets and blocks shall generally conform to The Recommended Connections Plan. The street and streetscape types allowed with the TOC District Area are depicted in the Roadway Types Illustration. All parcels of land shall have sidewalk connections that lead to transit stops.

An urban grid is intended to emerge from the redevelopment of the TOC and primary block faces should be created by constructing new streets and connecting existing streets. The maximum primary block face shall be based on neighborhood character and TOC District. For TOC-N and TOC-S Districts, the maximum primary block face is seven-hundred (700) feet and total block perimeter is twenty-one hundred (2,100) feet. Industrial development within the TOC-C District may exceed these maximums by providing the required Public Open Space and maintaining a block size with the potential for future roadways to subdivide the development.

Sidewalks or Urban Greenways adjacent to State Road 7/U.S. 441 may be required to be wider than the minimums required in Section 12-32.619 (Connectivity) if any right-of-way reservation dedications or easements have been previously granted to the Florida Department of Transportation.

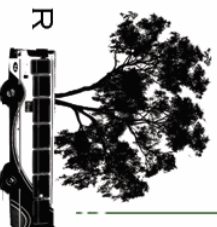




## A STREETSCAPE (continued)

The placement of lighting throughout the TOC District shall be designed as follows:

- (a.) Streetscape lighting is required for all streets except Local Links (A-3).
  - (b.) All light poles shall be located a minimum of four (4) feet from the curb face and maintained plumb and secure. The placement of poles shall not create obstructions to pedestrian ways.
  - (c.) Fixtures along Transit Oriented Corridors (R-7) and Connectors (A-5) shall be located approximately forty (40) feet on center. Required lighting fixtures along all local and primary roads shall be located approximately one hundred (100) feet on center.
  - (d.) Pedestrian lighting must establish a safe and secure atmosphere for nighttime use and provide a distinct ambiance, which differentiates pedestrian facilities from adjacent vehicular zones. The fixtures, poles and bollards must have a human scale and offer a visual appearance, which complements the other streetscape elements.
  - (e.) Lighting for landscape material shall not exceed between 0.5 and 1.0 ambient foot-candles. Landscape accent lights shall be unobtrusive in appearance and/or hidden from view. All landscape lighting shall be placed where beams are directed away from pedestrians' viewpoint. Lights mounted directly in trees shall be discouraged unless means of attaching the fixture and conduit are sensitively handled to protect the plant's health and assure a pleasing appearance.
4. TRASH RECEPTACLES: Consistent trash receptacles shall be located within the Public Streetscape and their location shall be determined at the block scale. A minimum of two (2) trash receptacles are required per block and their location shall serve transit facilities, Open Spaces and bench seating areas.



## LANDSCAPE

The landscape is the combined result of both natural processes and human activities. Underlining the value of landscapes for the daily life of people, the TOC will utilize the public realm of landscapes as a major component of sustainable development. The landscape within the TOC will be based on a balanced and compatible relationship between social needs, economic activity and the environment. All development within the TOC shall utilize Florida Friendly landscapes, including native and drought tolerant plant materials.

### A STREETSCAPE

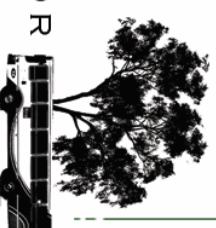
Pedestrian comfort and safety is critical to the success of the Transit Oriented Corridor. By focusing on the experience of the individual throughout the TOC districts, a more appropriate balance between pedestrian, bicycle and vehicular traffic can be realized. Streetscapes within the TOC must offer protection from the vehicular traffic as well as the elements. Understanding the diverse mix of uses within the TOC, it is also important to maintain adequate buffers to mitigate undesirable impacts on adjacent properties. All permanent and temporary structures within the public streetscape shall not impede the flow of pedestrian or bicycle traffic.

1. **TREES:** A minimum of one tree shall be planted within the First Lot Layer Frontage for each 30 feet of frontage line as depicted in "Layers" Illustration. Street trees shall consistently include the use of Live Oak (*Quercus virginiana*) and shall be located as identified in "Urban Greenway" or "Sidewalk" Illustrations.

2. **SEATING:** A minimum of two public benches shall be installed within the Public Streetscape along all roadways except Local Links (A-3). Bench locations shall be determined at the block scale and accommodate transit stops in addition to the Open Space requirements of the TOC.

3. **LIGHTING:** Pedestrian scaled lighting shall be installed within the Public Streetscape and within Open Spaces within the TOC. Lighting systems shall establish a sense of security and unity throughout the TOC District. The following minimum average maintained foot-candle levels shall be provided for the TOC District:

- a. Approach roadways: 1.0 FC.
- b. Side streets: 0.8 FC.
- c. Parking lots: 1.0 FC.
- d. Promenades: 1.0 FC.
- e. Sidewalks: 0.6 FC.
- f. Landscape lighting (varies).
- g. Property line spill: 0.5 FC.





## B SCREENING & BUFFERS

In order to protect the public spaces and adjacent mixed uses within the TOC from potentially negative impacts, it is important to ensure adequate mitigation measures are taken. Utilizing a combination of landscape buffers, berms and walls where appropriate shall ensure that all permitted uses within the TOC maintain compatibility throughout the districts. The TOC will continue to benefit from a diverse mix of uses as redevelopment progresses and residential and research and development uses are accommodated. In order to ensure that these uses maintain compatibility with the existing and continued industrial uses, screening and buffers will be required to be of significant quality to provide the necessary mitigation.



Landscape Buffers in the first lot layer shall be used to screen parking or vehicle storage in the second or third lot layers. These buffers may maintain access to parking from public sidewalks or passages. (See Lot Layers in definitions)



Bermed Landscape Buffers in the first lot layer shall be used to screen parking or vehicle storage and Limited Impact Industrial. These buffers may maintain access to parking from public sidewalks or passages where appropriate.



Walls with Bermed Landscape Buffers in the first lot layer shall be used to screen vehicle storage and Intensive Industrial uses. These buffers shall not maintain access to from public sidewalks or passages.

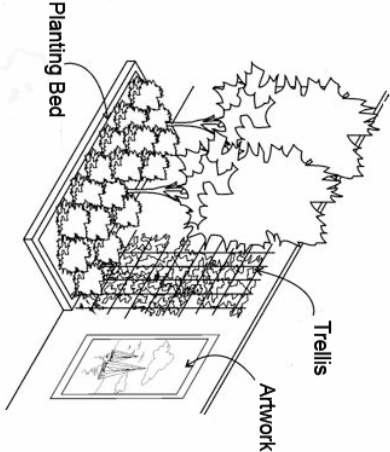


C WALLS

Walls shall be utilized when necessary to screen views and mitigate industrial related and potentially negative impacts on adjacent properties and the public realm. In TOC districts where property frontages allow the use of a wall to maintain a consistent urban form shall be subject to the regulations of this section. Emphasis on frontages with walls shall be placed on quality of design and construction. Structural and aesthetic articulation shall be required in order to ensure a consistent pedestrian experience with adjacent building and landscape frontages.

Walls shall be designed, installed and maintained to reduce the visual impact of large, undifferentiated frontages. Articulation and/or fenestration shall be used to visually minimize the scale and length of frontage walls. No blank walls will be permitted along frontages within the TOC.

Frontage walls, where permitted, shall not exceed eight (8) feet in height unless designed to screen Extensive Impact Industrial Uses. Frontage walls that extend for a distance greater than 100 linear feet, shall use an indented wall plane of 6-8 feet in depth and 30-40 feet in length per 100 linear feet of wall. The spaces created by indented walls shall be utilized for additional landscaping or streetscape amenities such as benches or integrated bus shelters.

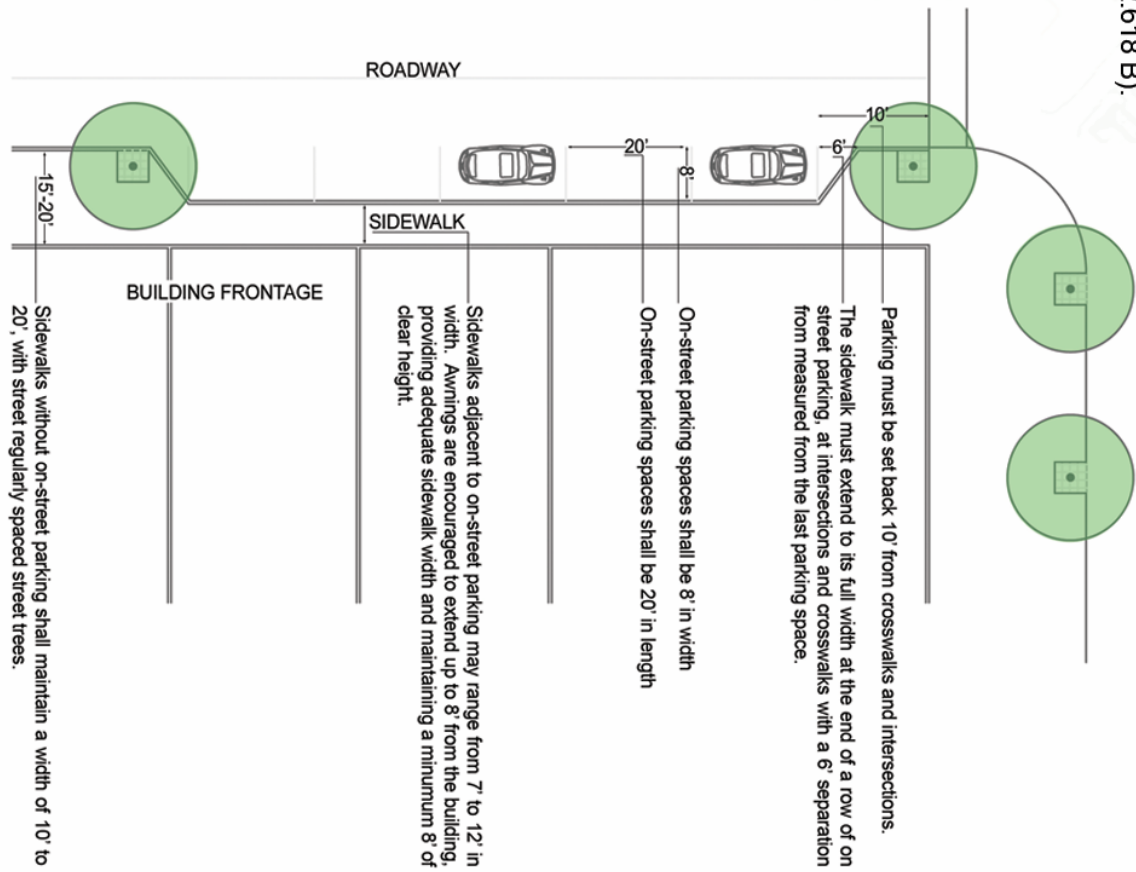




D

SIDEWALKS

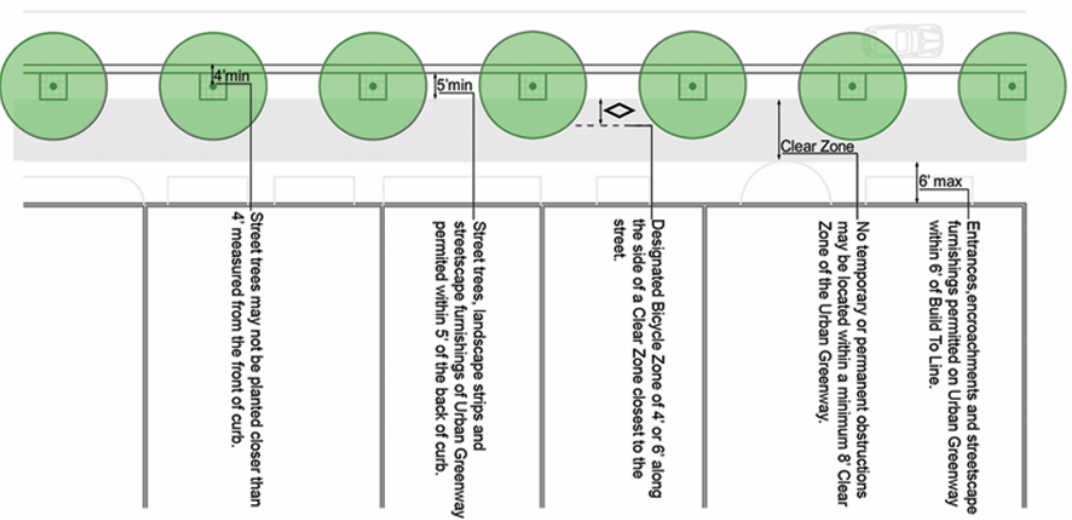
Sidewalks within the TOC district shall connect within an overall multi-modal network with a focus on access to transit routes and stations. The character and layout of all sidewalks shall conform to the TOC District it is constructed within and roadway type it is connected to and within the dimensions outlined in the Roadways Section (12-32.618 B).



D	SIDEWALKS (continued)
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Sidewalks within the TOC district along Transit Oriented Corridors shall conform to the following diagram and within the dimensions outlined in the Roadways Section (12-32.618 B).

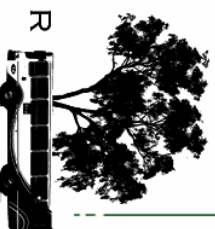
URBAN GREENWAYS shall be utilized along Transit Oriented Corridors and maintain a multi-modal function including a bicycle lane if one is not currently designated within the roadway.

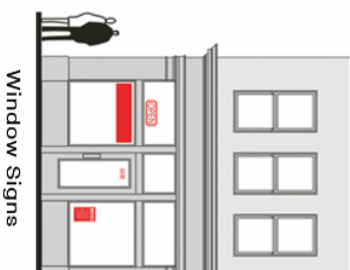
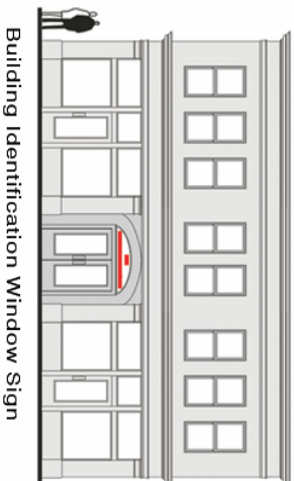
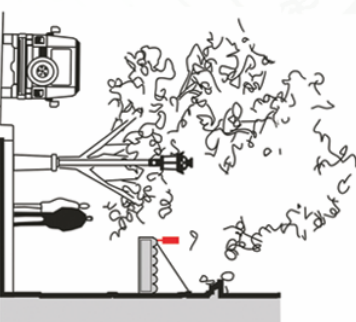
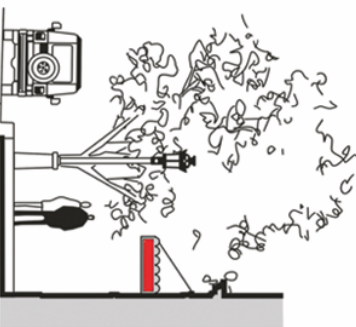
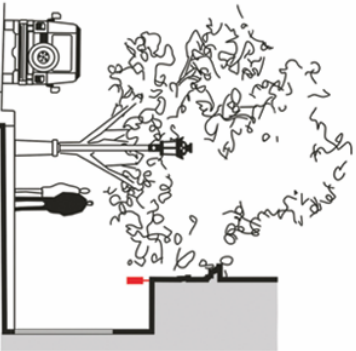
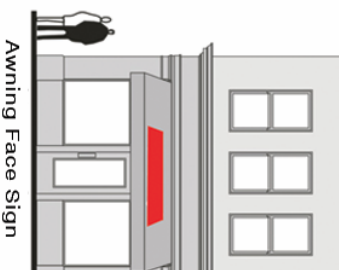
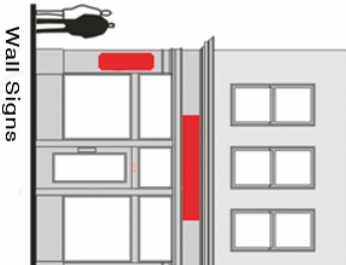
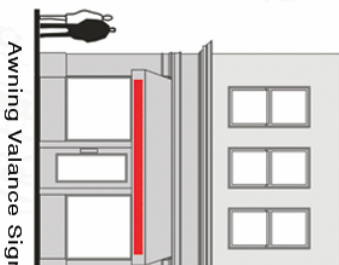
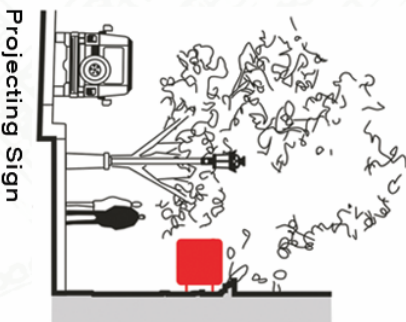
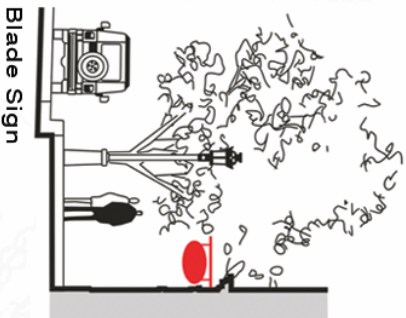
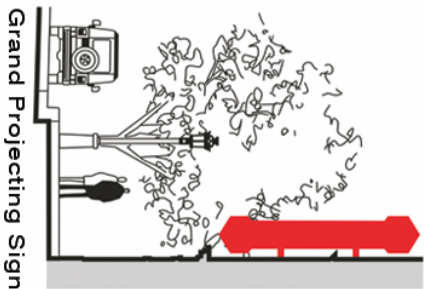




**SIGNAGE****Signage Standards**

1. One address number no more than six (6) inches vertically shall be attached to the building in proximity to the principal entrances, including front and rear or at a mailbox.
2. One blade sign for each business may be permanently installed perpendicular to the facade. Businesses with front and rear entrances shall be permitted one sign on each side of the structure. Such a sign shall not exceed a total of four (4) square feet in the TOC-TC District and six (6) square feet in the TOC-G, TOC-C and TOC-E Districts. (see "Signage" Illustration).
3. A single external sign band may be applied to the facade of each building, providing that such sign not exceed 3 feet in height by any length in the TOC-G, TOC-C and TOC-E Districts. (see Illustration 28, Signage).
4. Signage may be lit externally only with full-spectrum source, unless otherwise specified.
5. Grand Projecting signs or Building Identification and wall signs are only permitted on the building front along Transit Oriented Corridors and Connector roadways. See Recommended Connections Plan.
6. Wall signs are permitted on all building frontages except no external lighted signs are permitted facing residential uses.





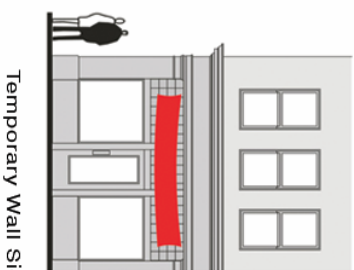
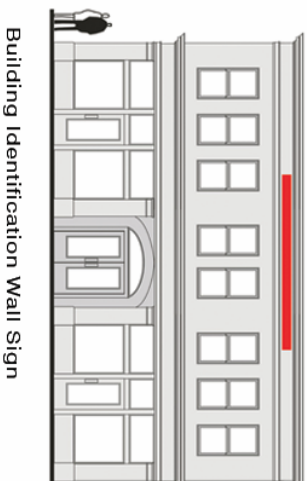
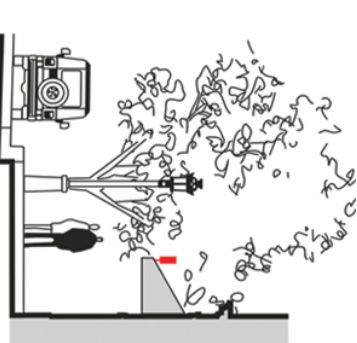
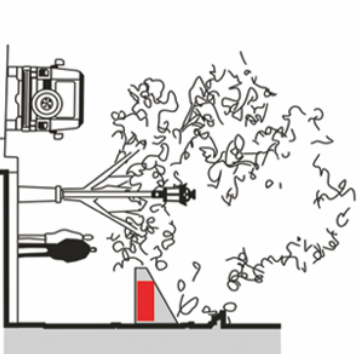
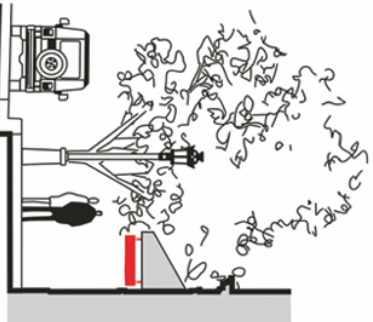
Canopy Facia Sign

Canopy Facia Sign

Above Canopy Sign

Building Identification Window Sign

Window Signs



Under Awning Sign

Awning Side Sign

Above Awning Sign

Building Identification Wall Sign

Temporary Wall Sign





## SIGNAGE (continued)

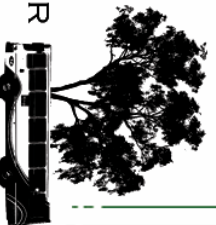
## High Rise Signage

Signs are permitted flat against the face of, or not more than eighteen (18) inches from the face of the outside wall of any building and not extending more than eighteen (18) inches above the wall upon which it is placed and supported throughout its length by such wall. No protruding portion of such sign shall be nearer than nine (9) feet to a walk or any area where there is pedestrian traffic; nor shall it extend beyond the wall in a horizontal direction; provided, however, that a sign placed on a mansard fascia shall be permitted to be erected vertically if the bottom section of this sign does not extend more than eighteen (18) inches from the mansard fascia.

If a sign is to be located within sixty (60) feet of ground level, then it shall be no larger than ten percent (10%) of the wall area upon which it is to be erected or one hundred twenty (120) square feet, whichever is less.

If a sign is to be located between sixty-one (61) feet and one hundred (100) feet above ground level, then it shall be no larger than ten percent (10%) of the wall area upon which it is to be erected or two hundred (200) square feet, whichever is less.

If a sign is to be located over one hundred (100) feet above ground level, then such a sign shall be no larger than ten percent (10%) of the wall area upon which it is to be erected or three hundred (300) square feet, whichever is less.



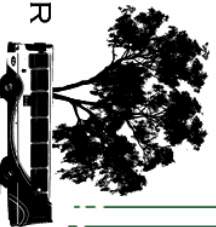
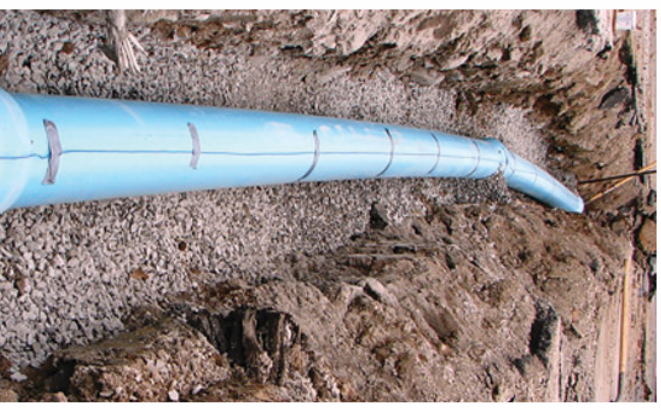
UTILITIES

**A** UNDERGROUND

Within the TOC-TC district, all utilities, including telephone, television cable, and electrical systems, shall be installed underground. Primary facilities providing service to the site and high-powered tension lines may be exempted from this requirement. The Town of Davie will develop a plan for the utility locations and development within the TOC districts will fund the installation. Within the development of site plans, large transformers shall be placed on the ground and contained within pad mounts, enclosures or vaults. The developer shall provide adequate landscaping with shrubs and plants to screen all utility facilities permitted aboveground. All transformers shall be screened in accordance with the requirements of this Code.

**B** EASEMENTS

Existing power line easements within the TOC are underutilized and maintain significant potential for investment. An evolution of the existing uses within the FPL easement area of the TOC shall be encouraged. Compatible uses within utility easements shall meet the requirements of the utility owner and be subject to the standards of this section. Where feasible, the use of power line easements may include greenways, blueways, park-and-ride facilities, vehicle storage or other compatible uses.





## OPEN SPACE

The Town of Davie Comprehensive Plan adopts a Level of Service (LOS) Standard of ten (10) acres of Recreation and Open Space for every 1,000 residents. The TOC shall include a minimum of 120 Acres of Open Space. This goal will be accomplished by the following open space standards and computations:

Lots less than three-hundred (300) feet of frontage shall provide 10% of the lot area in open space requirements.

Lots greater than three-hundred (300) feet of frontage shall provide a minimum of fifteen (15) percent of the primary frontage as open space in the first and/or second lot layer.

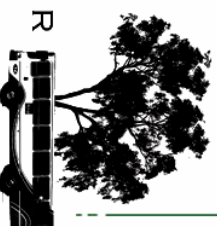
Lots or new blocks greater than two-hundred thousand (200,000) square feet shall provide at least one (1) acre of open space subject to the provisions of the Town of Davie Zoning Code and The Civic Space Index. Portions of the public sidewalk in private ownership may be counted as open space.

### A GREENWAYS

Greenways within the TOC Districts shall be planned in a centralized and connected system in order to serve as both a recreational amenity to the community and a multi-modal form of transportation for pedestrians and bicyclists to access transit. Alternative greenway opportunities shall be explored as redevelopment occurs including within utility easements or adjacent to blueways. Physical boundaries and edges within the TOC Districts shall be designed to incorporate greenway connections when feasible. Development within the TOC will be required to conform to, and contribute to the implementation of, Open Space and Stormwater master plans.

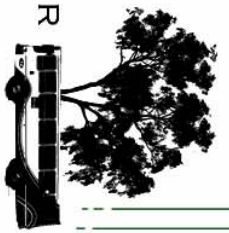
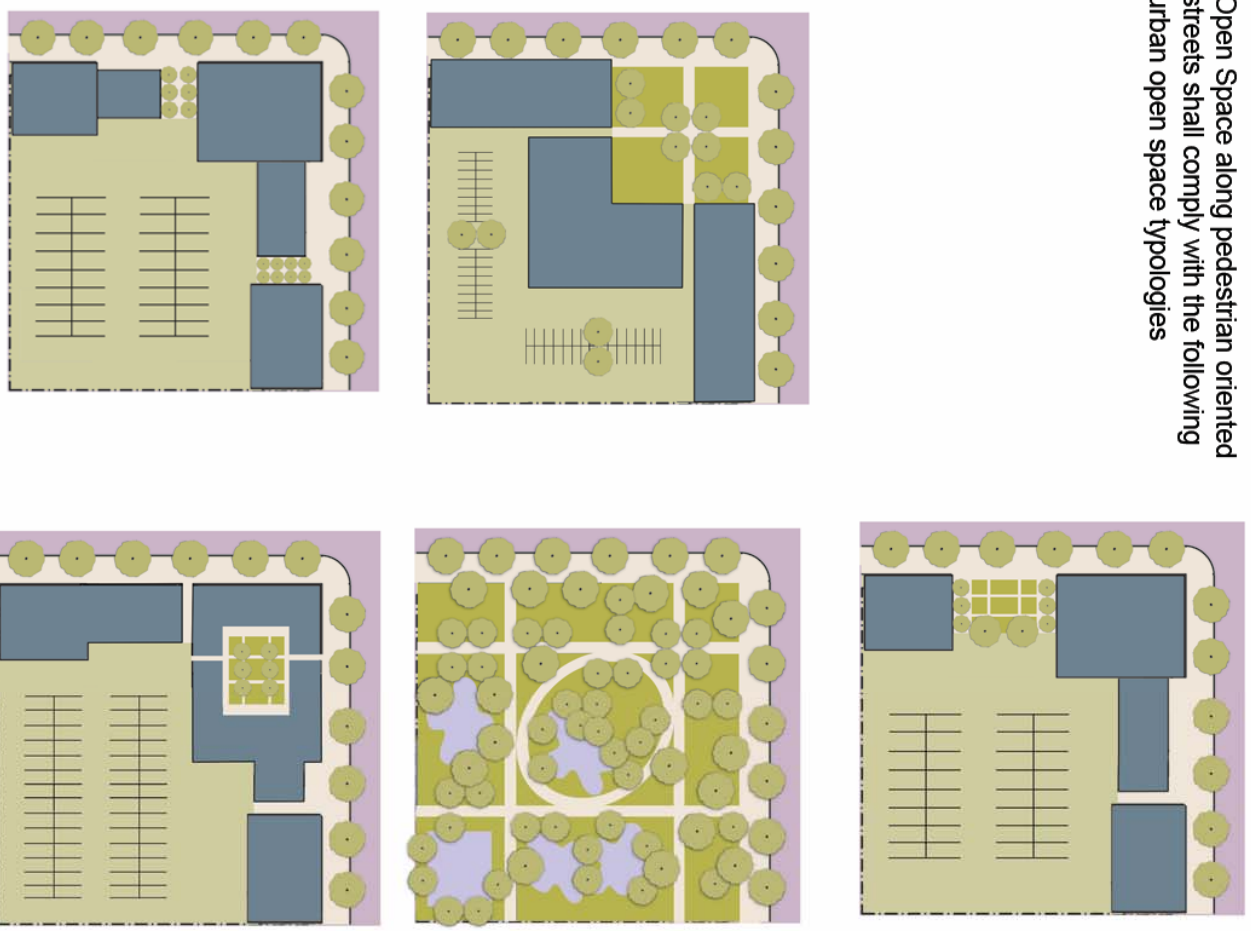
### B BLUEWAYS

Blueways within the TOC Districts shall be planned in a centralized and connected system in order to serve as both an amenity within an open space system, but also a component functional and sustainable stormwater management system. In accordance with the Town of Davie Stormwater Master Plan, all blueways shall maintain water quality and serve to connect into an overall drainage system. Whenever possible, all blueways shall be utilized as a recreational and aesthetic resource to the TOC district in conjunction with greenways and other recreational facilities. Water bodies within the TOC shall be designed as a focal point and access to them shall be maintained.



OPEN SPACE (continued)

Open Space along pedestrian oriented streets shall comply with the following urban open space typologies





**D****ENERGY EFFICIENT BUILDING STANDARDS**

All design and construction within the TOC shall make use of passive energy conservation measures including proper building orientation; landscape shading of buildings and pedestrian areas; and reflective wall and roof surfaces for non-air-conditioned container storage areas. All buildings shall achieve Leadership in Energy and Environmental Design (LEED) certified standards.

Incentives for exceeding minimum LEED Certification:

All: Expedited site review and permitting process

Silver: Exceed height requirements by 1 additional story.

Platinum: Exceed height requirements by 2 additional stories and reduce parking requirements by 5%.

Gold: Exceed height requirements by 3 stories and reduce parking requirements by 10%.



SUSTAINABILITY

**A** STORMWATER MANAGEMENT

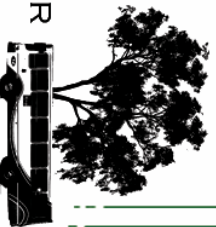
The Town of Davie has developed a Stormwater Master Plan as a holistic approach to conservation and watershed hydrology including provisions for jurisdictional wetlands. All development within the TOC shall conform to the standards and regulations outlined in the Stormwater Master Plan and utilize Low Impact Design (LID) Low Impact Development (LID), a stormwater management approach with a basic principle that is modeled after nature: manage rainfall at the source using uniformly distributed decentralized micro-scale controls. The goal of all development within the TOC is to mimic a site's predevelopment hydrology by using design techniques that infiltrate, filter, store, evaporate, and detain runoff close to its source. Development within the TOC shall address stormwater through small, cost-effective landscape features located at the lot level. These landscape features, known as Integrated Management Practices (IMPs), shall be utilized as the primary tool for stormwater management within the TOC. Open space, rooftops, streetscapes, parking lots, sidewalks, and medians shall all function collectively within an overall stormwater management system.

**B** WATER REUSE

Irrigation systems within the TOC shall use purple pipe and contain all of the necessary infrastructure to allow for future reclaimed water use. Florida Friendly Landscapes shall be utilized in order to minimize irrigation demand

**C** EDUCATION AND OUTREACH

In an effort to promote the sustainable development within the TOC Districts, the Town Shall develop and implement an outreach program to educate residents within the area. Developments within the TOC shall be required to work within the Town's programs and demonstrate the importance of maintaining energy efficient and sustainable communities.





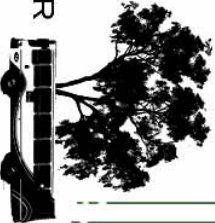
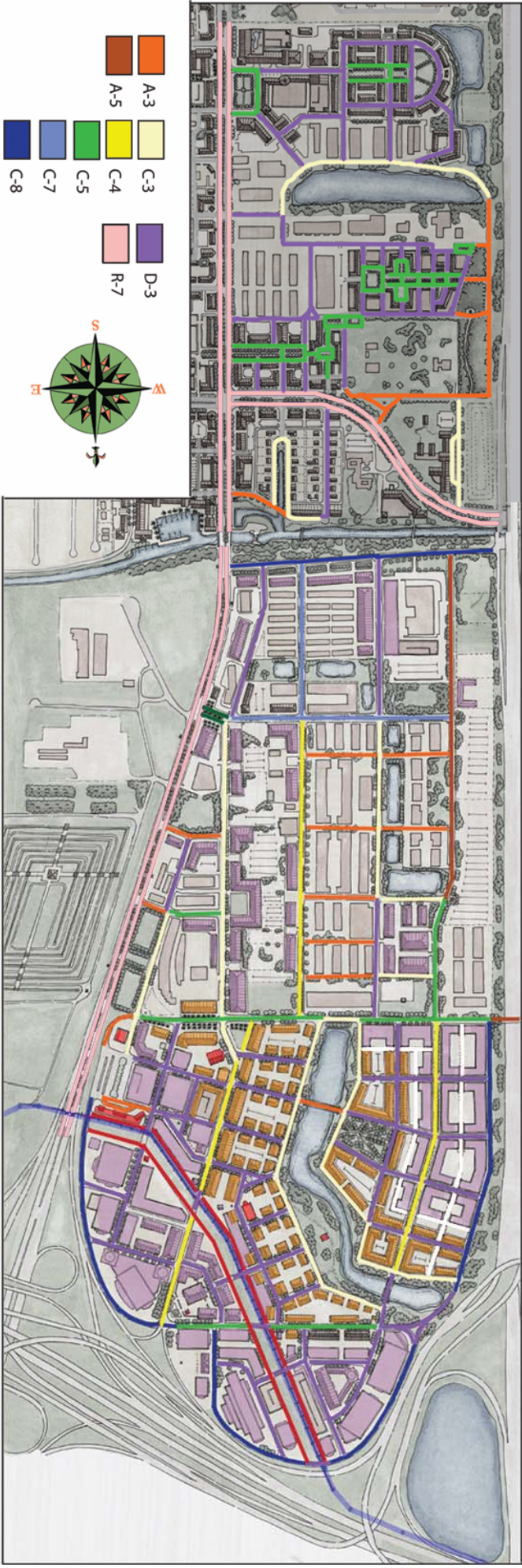
Section  
12-32.619

CONNECTIVITY

Access to transit and a reduction of vehicle miles traveled within the TOC District is dependent upon an efficient and multi-modal transportation system. Roadways within the TOC shall generally conform to the Recommended Connections Plan. Existing roadways shall be utilized and enhances where possible to meet the standards of this section. All development within the TOC shall ensure that each parcel of land have sidewalk connections that lead

A RECOMMENDED CONNECTIONS PLAN

The development of a road network and system of pedestrian and bicycle facilities shall generally conform to the Recommended Connections Plan. The illustrative plan indicates proposed development patterns based on the 2004 State Road 7/U.S. 441 Charrette Master Plan.



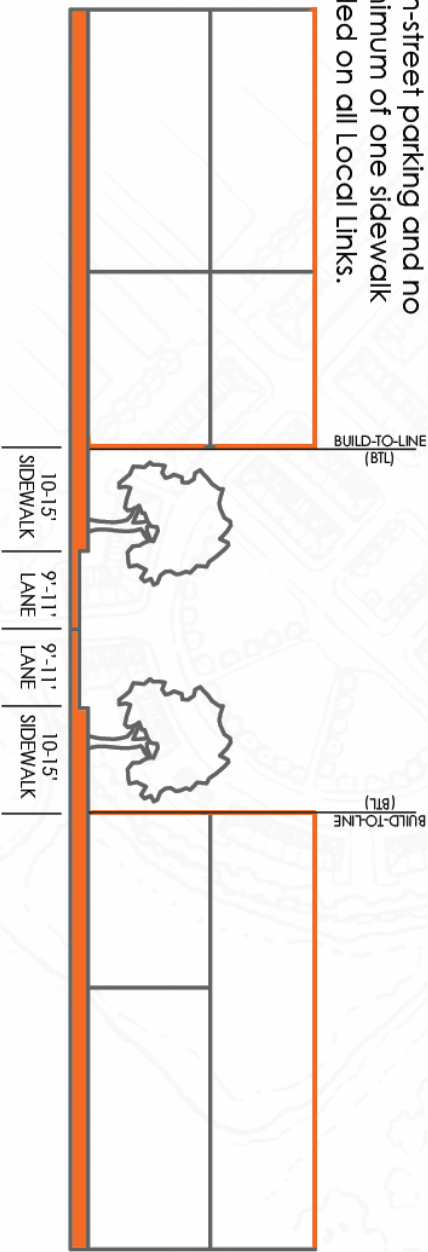


**B** ROADWAY TYPES

The design of all streets and blocks within the TOC District shall be transit oriented with a primary focus on the pedestrian experience. Tree lined roadways with on-street parking and wide sidewalks shall be constructed according to the Recommended Connections Plan.

**A-3** Local Links

These streets serve as neighborhood links without on-street parking and no median. A minimum of one sidewalk shall be provided on all Local Links.

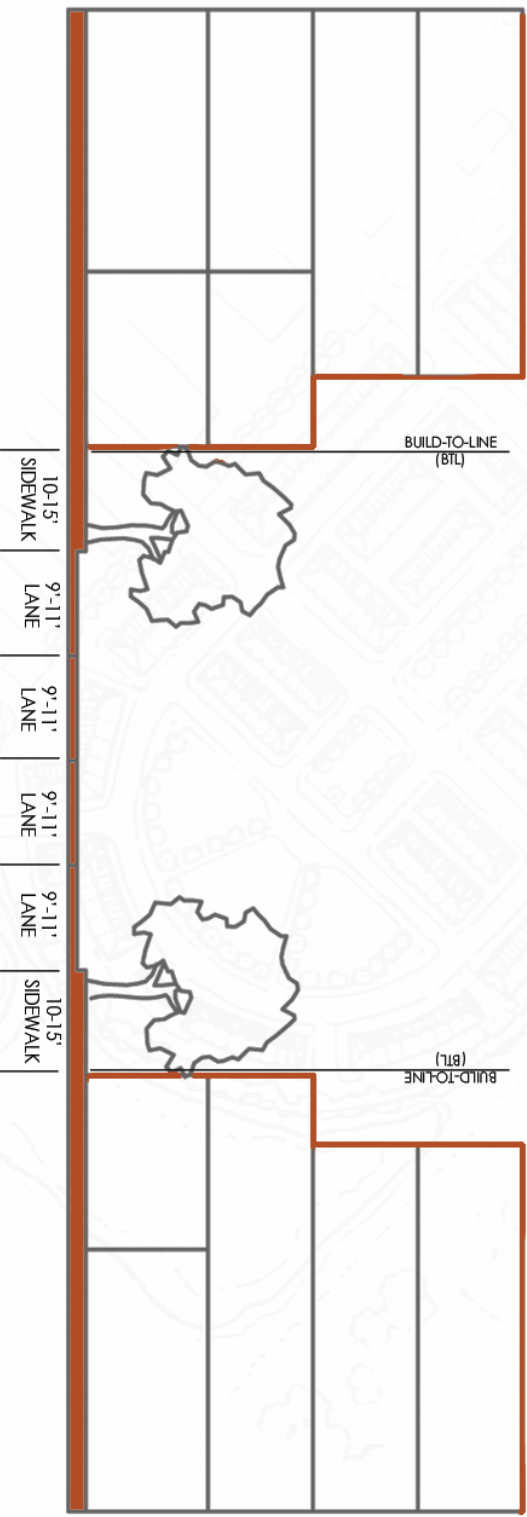




**B** ROADWAY TYPES (continued)

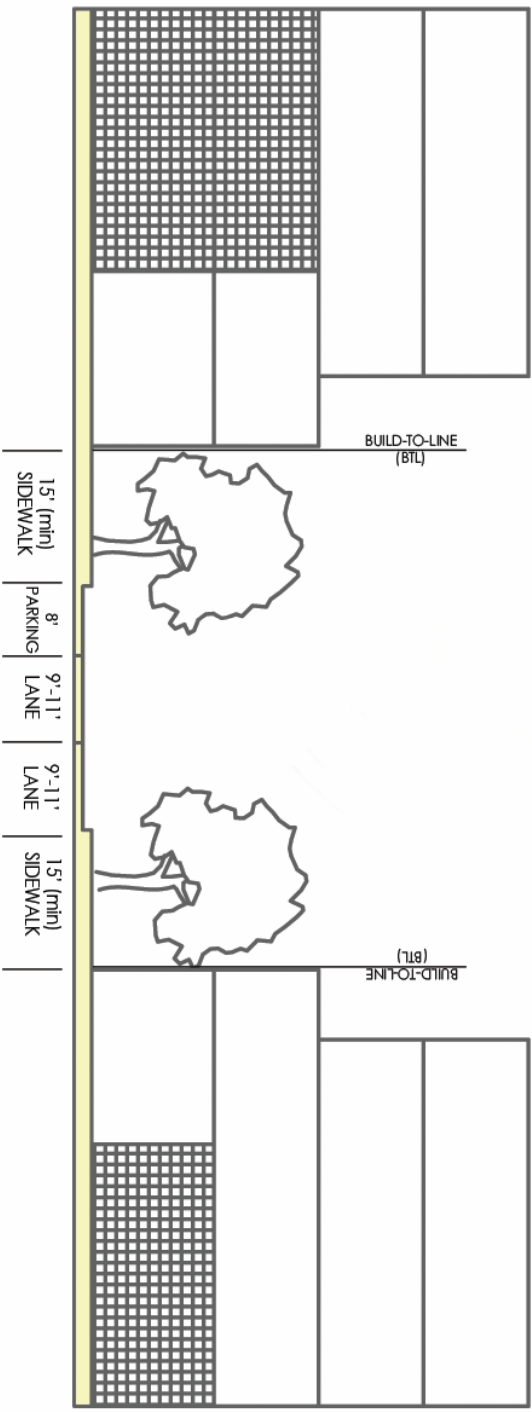
**A-5** Connectors

Connecting roadways without on-street parking and no median. A minimum of one sidewalk shall be provided on all Connectors.



**C-3** Local Roads  
Limited Parking

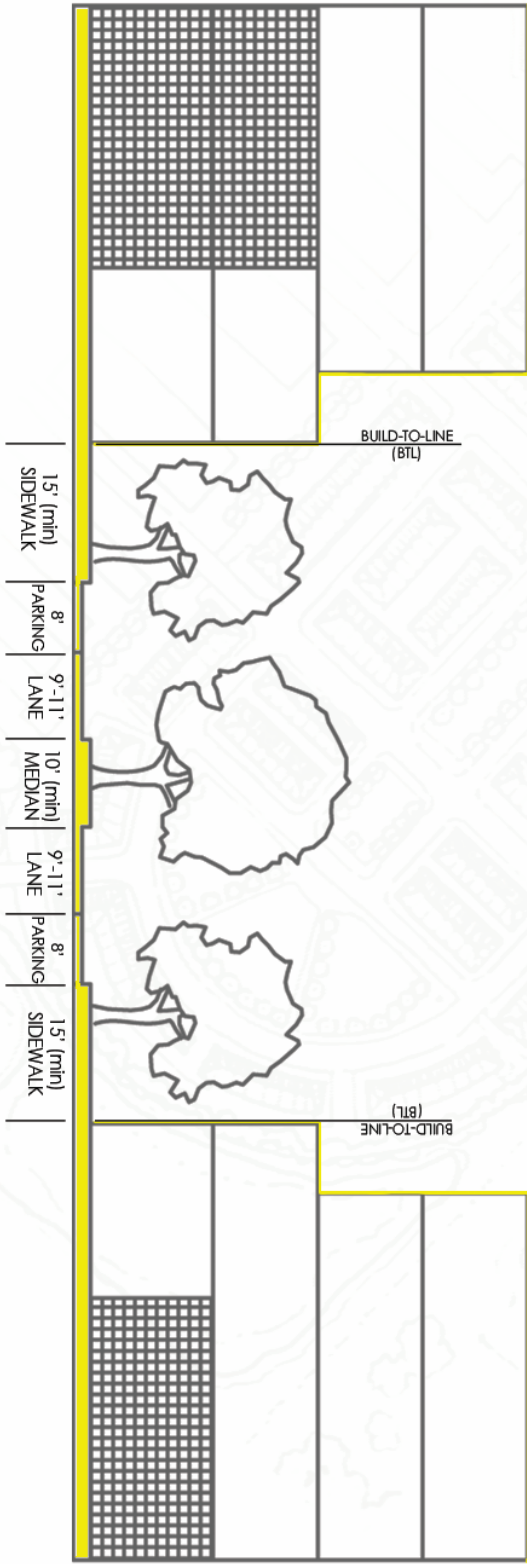
Local roadways with on-street parking on one side and no median. A minimum of one sidewalk shall be provided on all Local Roads.



**B** ROADWAY TYPES (continued)

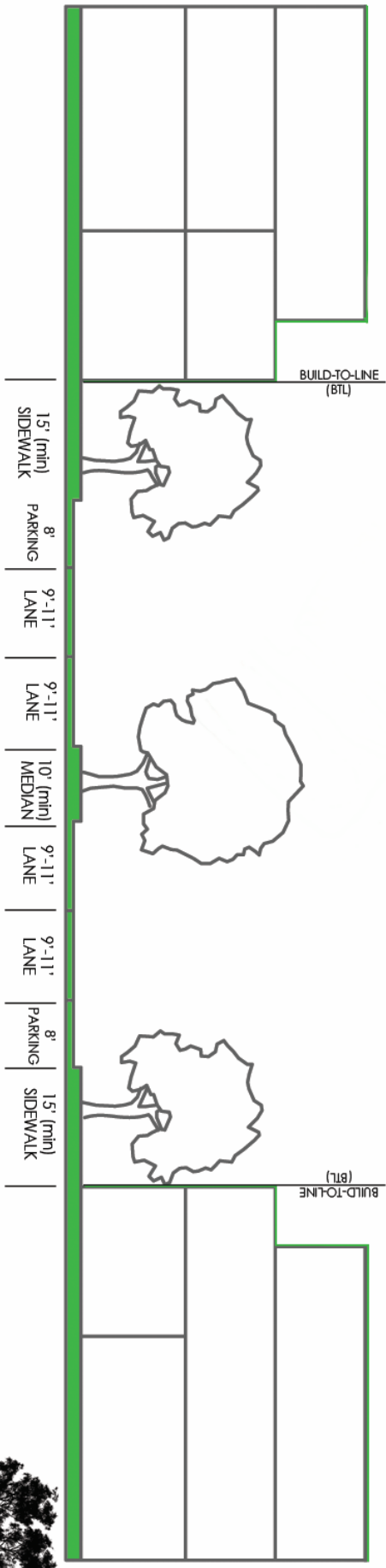
**C-4** Local Divided  
On-Street Parking

Local roadways with on-street parking on both sides and a median. Sidewalks shall be provided on all Local Roads.



**C-5** Primary Divided Roads  
On-Street Parking

Main Roads with on-street parking on both sides and a median. Sidewalks shall be provided on all Primary Roads.

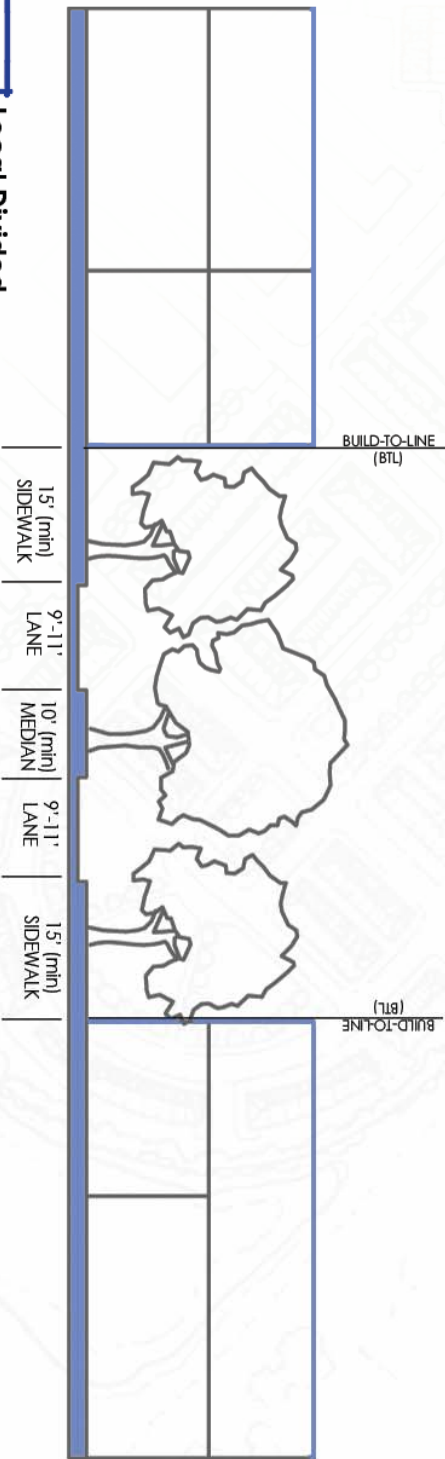




**B** ROADWAY TYPES (continued)

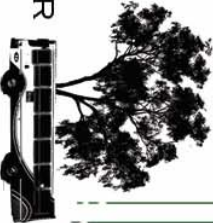
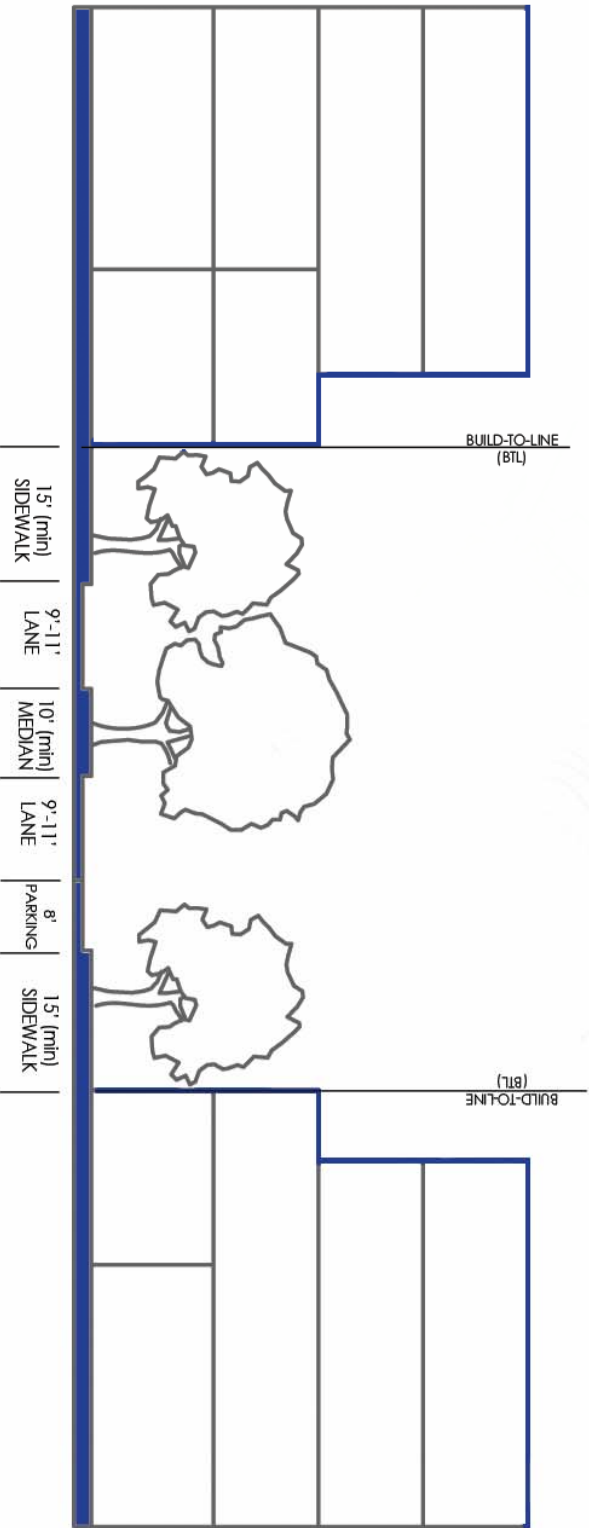
**C-7** Local Divided

Local roadways with a median and no on-street parking. Sidewalks shall be provided on all Local Divided Roads.



**C-8** Local Divided Limited Parking

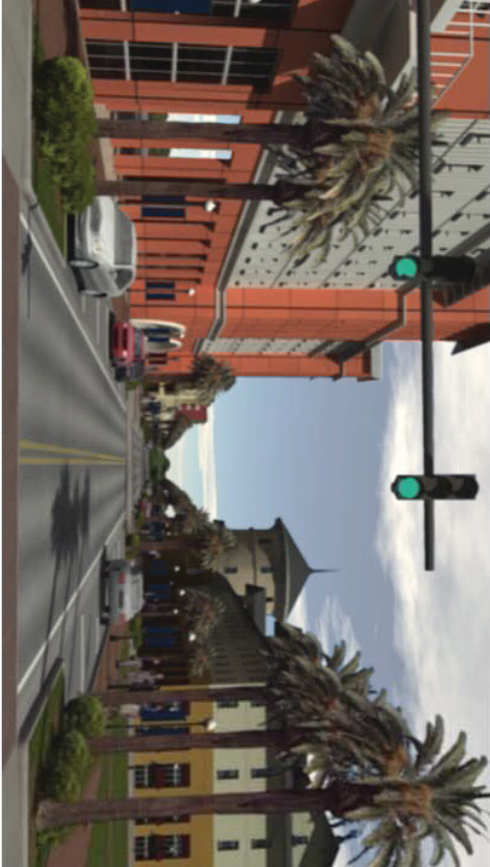
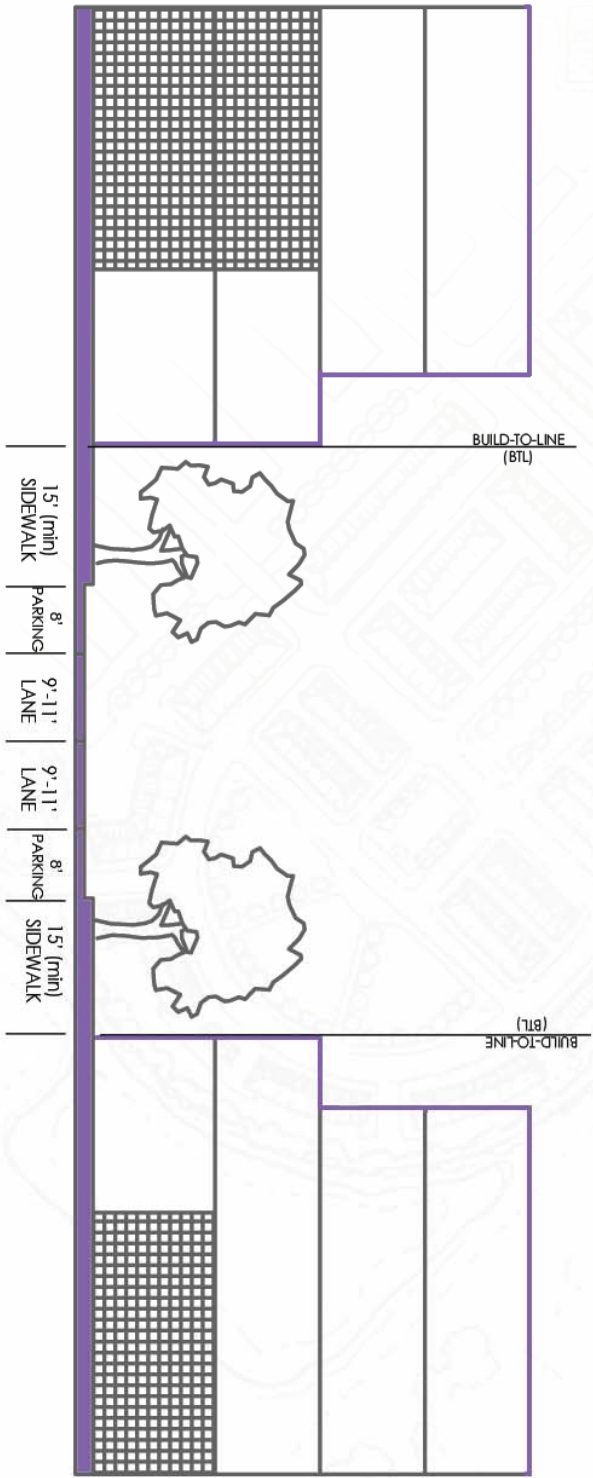
Local roadways with a median and on-street parking on one side. Sidewalks shall be provided on all Local Divided Roads.



B ROADWAY TYPES (continued)

D-3 Local Roads  
On-Street Parking

Local roadways with on-street parking and no median. Sidewalks shall be provided on all Local Roads.

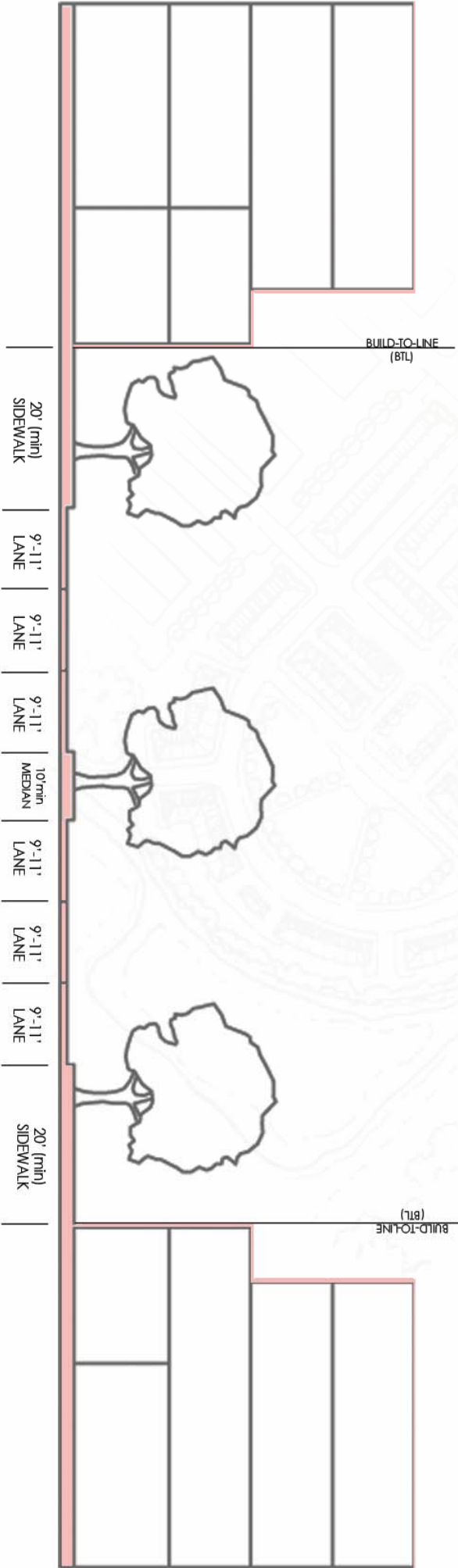




B ROADWAY TYPES (continued)

R-7 Transit Oriented Corridors

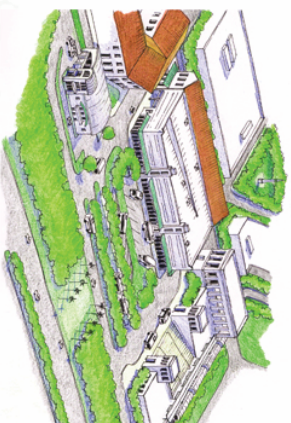
Divided highway with extended sidewalks to incorporate transit amenities. Exclusive Bus Rapid Transit Lanes or At-Grade Light Rail shall be accommodated within the 6-lane section.



## C ACCESS TO TRANSIT

Access to transit shall be the primary focus of all multi-modal connections within the TOC districts. The circulator system routes within the TOC shall operate in coordination with the Town of Davie and Broward County Transit to provide reliable and efficient transit service to all users within the district.

All internal transit circulation shall connect as indicated in the multi-modal plan to other transit services within the Town of Davie and regional connections through Broward County Transit.



Broward County Transit operates and maintains local and express bus service along State Road 7/U.S. 441. Local route 18 consistently maintains the highest ridership throughout the county along the corridor and serves as the basis for transit-oriented development within the TOC districts. The express service along the corridor is identified as the 441 Breeze with limited stops along State Road 7/U.S. 441, one located at the intersection of the 441 Corridor and Griffin Road.

All buses within the system are equipped with bike racks to assist riders in using alternative modes of connecting to transit facilities.

The State Road 7/U.S. 441 Corridor has been identified as a Major Regional Facility in the Regional Long Range Transportation Plan developed by the Southeast Florida Transportation Committee (SEFTC) and holds the distinct potential of evolving into a Bus Rapid Transit (BRT) system with dedicated transit lanes and eventually a Light Rail or other fixed guideway transit system.

The State Road 7/U.S. 441 Strategic Master Plan includes provisions for the eventual implementation of rail along the corridor and redevelopment within the TOC shall also provide for the infrastructure, densities and intensities necessary to facilitate rail transit. Transit stop facilities shall be incorporated into all site development plans located along existing or future transit routes. Parking requirements shall be dependent on the provision of transit shelters as indicated in Section 12-32.611 D Parking Calculations.



Development within the TOC shall be subject to the provisions of the Town of Davie Areawide Development of Regional Impact and its' associated development order conditions which specify the direct transit fee payment as a proportionate fair share of development within each district.





Section  
12-32.620

ART IN PUBLIC PLACES

Any permanent or temporary creative works in any discipline, primarily, but not necessarily, those of professional artists, placed or incorporated in public places for the enhancement, enjoyment, or enrichment of human experience shall conform to the provisions of this section. New development or redevelopment projects within the TOC district, not including single-family or residential dwellings of less than 6 units, shall incorporate a public art plan in their design submissions and provide public art work(s) equal to 1% of the project's building permit values or contribute 1% of the project's building permit values to the Town's public art reserve fund or any combination thereof up to 1% of the project's building permit values.

Section  
12-32.621

AIRPORT NOISE MITIGATION

All development within the TOC shall utilize current research and technologies to mitigate noise levels and ensure the quality of life for the TOC districts. Although no residential or incompatible uses are permitted within the adopted 60 DNL contour according to the most recent Federal Aviation Administration (FAA) Noise Exposure Map, the Town will ensure the mitigation of noise impacts from the airport for residential uses within the 60-65 DNL contours. All new residential units in the TOC that fall within the 60 DNL noise contour and up to but not including the 65 DNL noise contour shall be constructed to achieve outdoor-to-indoor Noise Level Reduction (NLR) of at least twenty-five decibels (25 dB) to thirty decibels (30dB).

Developers of residential properties within the 60 DNL to 65 DNL shall provide documentation of the NLR and an executed commitment to provide notice to all purchasers of such residential housing that the property lies within the 60 DNL to 65DNL.



Section  
12-32.622

SCHOOLS

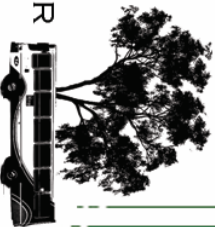
School siting opportunities exist throughout the TOC within the context and character identified for each district. Compact urban school siting within the TOC shall be coordinated through the Broward County School District (BCSB) and conform to the intent of this section. The Town will work with the BCSB to develop strategies and conceptual design approaches for future school development needed to accommodate existing and future student populations as identified by the Broward County School Board.

The existing limitations to the amount of vacant land needed to construct new schools near existing and planned development has caused future school siting to be accommodated within existing urban areas. Educational facility development within the TOC shall identify alternative and innovative solutions necessary to continue to provide high quality school facilities for existing and future student populations. Existing and future uses shall be evaluated for opportunities of collocation and joint uses, such as recreational facilities, workforce housing, infrastructure, and community centers.

Section  
12-32.623

PUBLIC PURPOSE

In the interest of furthering the goals outlined in the Town of Davie State Road 7/U.S. 441 Corridor Master Plan, the Town will recoup funds utilized for the improvement and economic development of the TOC. The dedication of land within the TOC may be required for the siting of utilities, transit infrastructure, pedestrian amenities, stormwater enhancements, open space, public schools and other aspects of the Master Plan necessary for successful implementation. All land dedications within the TOC shall be subject to the provisions of this section and meet the intent of the TOC.





See ARTICLE XIV Definitions in the Davie Code of Ordinances. When used in this chapter, the following terms shall have the following meanings herein ascribed to them:

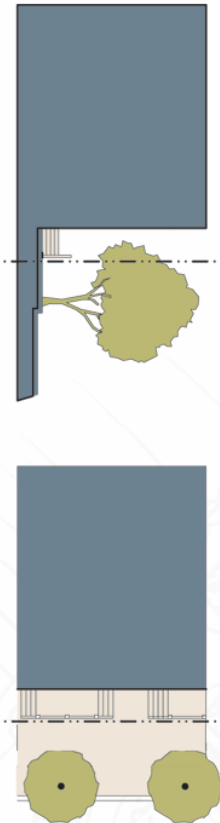
**Accessory unit:** An apartment not greater than 600 square feet sharing ownership and utility a single-family residence.

**Allee:** a regularly spaced and aligned row of trees usually planted along a road or pedestrian Path.

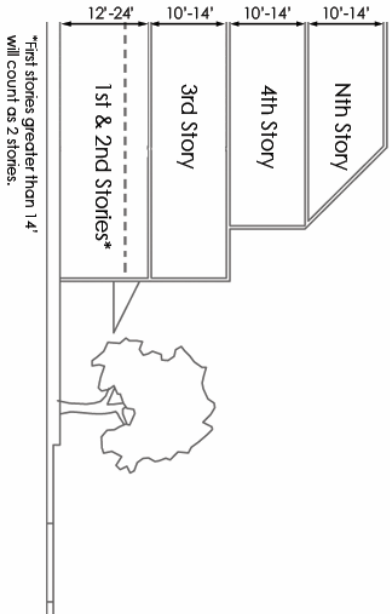
**Building Configuration:** the form of a building, based on its massing, private frontage, and height.

**Building Disposition:** the placement of a building on its lot. (Building Disposition Illustration).

**Building Frontage:** Are controlled by setbacks, which are regulated by the types of streets that they front.



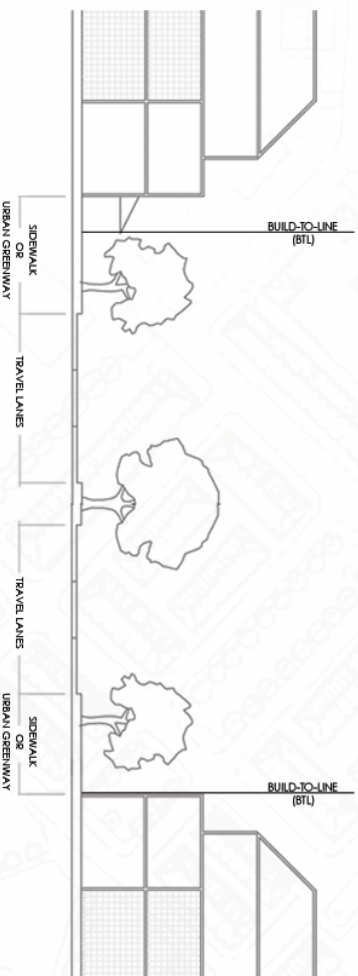
**Building Height:** The vertical distance measured from the grade upon which the building is placed to the highest point of the roof surface of a flat roof, to the deck line of a mansard roof, to the mean height level between eaves and ridge gable on hip and gambrel roofs, or for buildings set back from the street line, the mean vertical distance from the average elevation of the finished grade along the front of the building provided that this grade is not greater or less than the height of the grade above the established curb level.



DEFINITIONS (*continued*)

**Building Type:** A structure category determined by function, configuration, including frontage.

**Build To Line (BTL):** The abbreviation "BTL" shall mean Build-To-Line.



**Courtyard Building:** A building that occupies the boundaries of its lot while internally defining one or more private patios.

**Courtyards/ Outdoor:** Space that is located within the confines of building walls, sometimes between multiple developments.

**Edgeyard Building:** a building that occupies the center of its lot with setbacks on all sides.

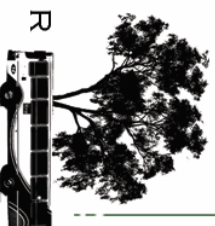
**Elevation:** an exterior wall of a building not along a Frontage Line. (see Building Disposition Illustration).

**Entrance, Principal:** the main point of access of pedestrians into a building.

**Extensive Impact:** A use having a high potential for negative impacts on the environment or on surrounding uses in terms of noise, glare, vibration, smoke, vapors, odors, fire or explosive hazards. Any industrial use where more than 20 percent of the use is conducted outside of an enclosed building, or involves the outdoor storage of materials, manufactured products or commercial vehicles exceeding 20 percent of the gross floor area of the all buildings on the lot, shall be deemed an extensive impact industry. This definition specifically excludes other separately defined uses referenced in these regulations, such as but not limited to mining, salvage yards, and solid waste disposal facilities.

**Frontage:** the privately or publicly held layer between the frontage line and the principal building facade. The structures and landscaping within the Private Frontage may be held to specific standards. The variables of Private Frontage are the depth of the setback and the combination of architectural elements such as fences, stoops, porches, galleries and may include urban greenways and sidewalks. (see Building Frontages Illustration).

**Gallery:** A frontage wherein the façade is aligned close to the frontage line with as attached cantilevered shed or a lightweight colonnade overlapping the sidewalk.





DEFINITIONS (*continued*)

**Green Building:** A sustainable building that emphasizes state of the art strategies for sustainable site development, water savings, energy efficiency, materials selection and indoor environmental quality and is identified as needing verifiable Green Building standards.

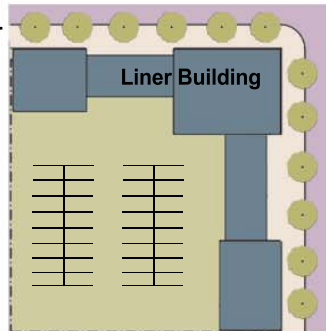
**Greenway:** an open space corridor in largely natural conditions which may include Trails for bicycles and pedestrians.

**Horizontal mixed-use (attached):** A mixed of uses whereby different uses are adjacent to each other within the same plot or development.

**Horizontal mixed-use (detached):** A mix of uses whereby different uses are adjacent to each other within the same lot or development.

**Layer:** a range of depth of a lot within which certain elements are permitted. The maximum depth of the first lot layer varies and is the distance between the curb and the required built-to-line, commonly referred to as the front sidewalk in this Code. The minimum depth of the first lot layer is twenty-five (25) feet and may vary by parcel. Additional depth may be required to satisfy Right-of-Way reservation requirements. No first lot layer exceeds fifty (50) feet in depth. The minimum depth of the second lot layer is twenty (20) feet and a maximum depth of thirty (30) feet. The third layer is the area between five (5) feet of the rear property line and the maximum depth of the second layer.

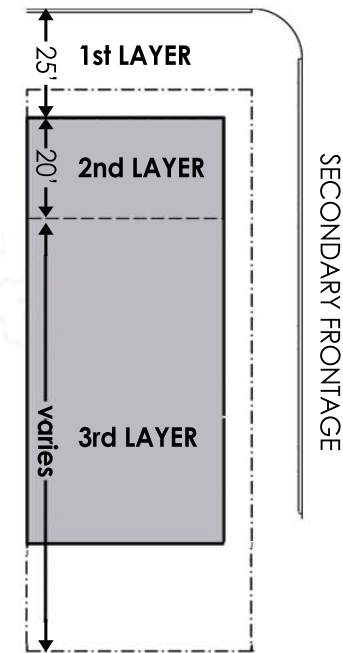
**Liner Building:** A building usually shallow in depth, specifically designed to mask a parking lot, parking garage or blank wall from a frontage.



**Live work unit:** A dwelling unit that is also the primary place of work, which place of work is located on the ground floor for the occupant of the unit.

**Lodging:** premises available for daily and weekly renting of bedrooms. The area allocated for food service shall be calculated and provided with parking according to retail use.

PRIMARY FRONTAGE



DEFINITIONS (*continued*)

**Lot frontage:** The distance that a lot abuts a street.

**Mixed Use:** multiple functions within the same building through superimposition or adjacency, or in multiple buildings within the same area by adjacency. Mixed use is one of the principles of Traditional Neighborhood Design (TND) development from which many of its benefits are derived, including compactness, pedestrian activity, and parking space reduction.

**Office:** premises available for the transaction of general business but excluding retail, artisanal and manufacturing uses.

**Open Space:** an outdoor area dedicated for public use. Open Space types are defined by the combination of certain physical constants including the relationship between their intended use, their size, their landscaping and their enfronting buildings. (Open Space Types Illustration).

**Outbuilding:** An accessory building that does not exceed six hundred (600) square feet of habitable space, including a parking garage or carport.

**Parking Structure:** a building containing two or more stories of parking. Parking Structures shall have Liner Buildings at the first story or higher.

**Passage:** A pedestrian connector passing between buildings, providing shortcuts through long blocks and connecting rear parking areas to frontage, and which may or may not be roofed.

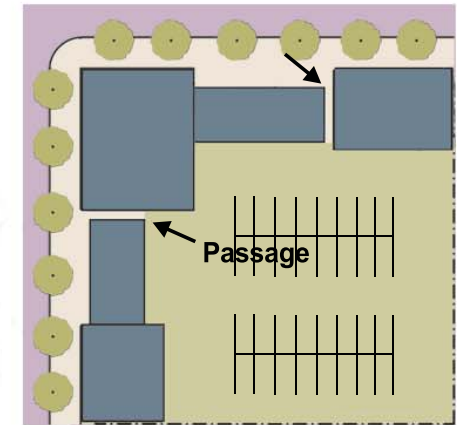
**Path:** a pedestrian way traversing a park or rural area, with landscape matching the contiguous open space. Paths should connect directly with the urban sidewalk network.

**Pervious:** Materials as a substitute to concrete and asphalt that allow for water infiltration.

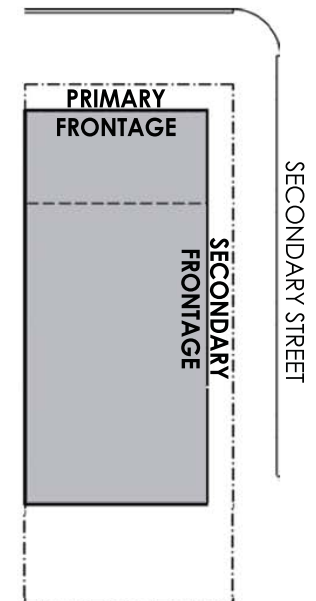
**Planter:** the element of the public streetscape which accommodates street trees. Planters may be continuous or individual.

**Plazas:** Large outdoor spaces that extend the public realm from the street or sidewalk to the main entry of an adjacent building. (see Illustration 10, Open Space Types).

**Primary-Secondary Frontage:** thoroughfare designations appearing on the Regulating Plan. Buildings on the Primary Frontage streets are subject to all of the provisions of this Code. Buildings on the Secondary Frontage Streets are exempt from certain provisions, allowing for Warranted open parking lots, unlined parking decks, drive-throughs and hermetic building fronts. (see Frontages Illustration).



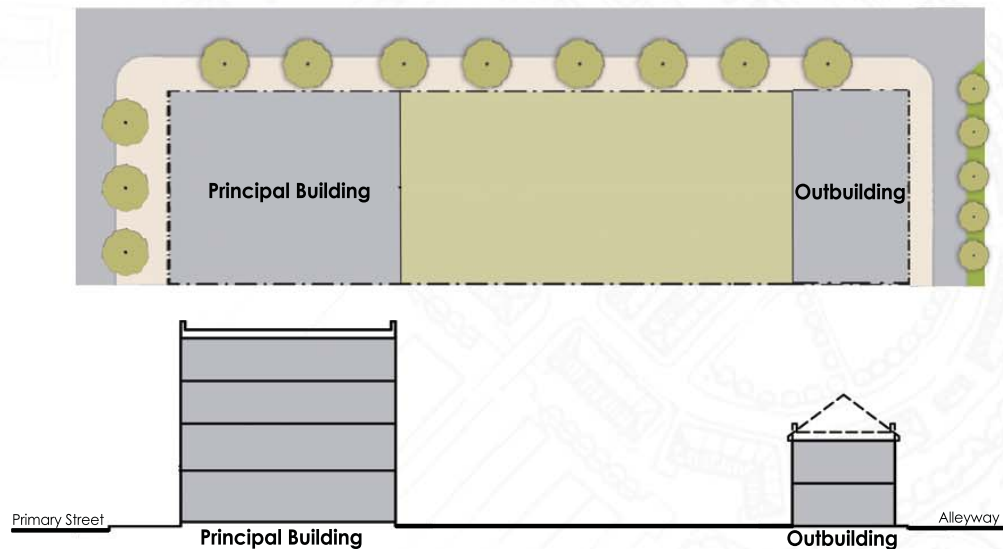
PRIMARY STREET





DEFINITIONS (*continued*)

**Principal Building:** the main building on a lot; the primary purpose for which a lot or building is intended to be used, usually located toward the frontage.



**Principle frontage:** Those lot lines that coincide with public footage on a collector or arterial roadway.

**Rear Alley:** a vehicular driveway located to the rear of lots providing access to service areas and parking, and containing utility easements. Alleys should be paved from building face to building face, with drainage by inverted crown at the center or with roll curbs at the edges.

**Rearyard Building:** A building that occupies the full frontage line, leaving the rear of the lot as the sole yard.

**Retail Frontage Line:** Frontage Lines designated on a Community Plan that require the provision of a Shopfront, causing the ground level to be available for retail use.

**Road:** a vehicular way, either existing or proposed, incorporating moving lanes and parking lanes within a right-of-way. (see Recommended Connections Plan).

**Rowhouse:** a single-family dwelling that shares a party wall with another of the same type and occupies the full frontage line. (Syn: Townhouse, see Rearyard Building).

**Shopfront:** A frontage wherein the façade is located at the back edge of the sidewalk (BTL) with a substantial glazing on the sidewalk level, and an awning or cantilevered shed roof projecting at least seven (7) feet forward of the BTL when the shopfront is not combined with a gallery or arcade frontage.



DEFINITIONS (*continued*)

**Sidyard Building:** a building that occupies one side of the lot with a setback to the other side.

**Sidewalk:** the paved layer of the public frontage dedicated exclusively to pedestrian activity. (see Sidewalk Illustration).

**Specialized Building:** a building that is not subject to Residential, Commercial, or Lodging classification. Most specialized buildings are dedicated to manufacturing and transportation, and are distorted by the trajectories of machinery.

**Standard Pedestrian Shed:** An area, approximately circular, that is centered on a Common Destination. A Standard Pedestrian Shed is one quarter (1/4) mile radius or one-thousand three hundred twenty (1,320) feet, about the distance of a five-minute walk at a leisurely pace. It has been shown that provided with a pedestrian environment, most people will walk this distance rather than drive.

**Stoop:** Frontage wherein the facade is aligned close to the frontage line with the lower story elevated at least eighteen (18) inches from the sidewalk to establish a sense of transition from public to private realm, and may be elevated sufficiently to secure privacy for the windows.

**Story:** A habitable level with in a building of no more than fourteen (14) feet in height from finished floor to finished ceiling, except that the first floor may be up to twenty-four (24) feet in height.

**Street Frontage:** The area where building faces streets and building meets with side walk.

**Streetscape:** The urban element that establishes the major part of the public realm. The streetscape is composed of roads (travel lanes for vehicles and bicycles, parking lanes for cars, urban greenways and sidewalks or paths for pedestrians) as well as the visible private frontages (building facades and elevations, porches, yards, fences, awnings, etc.), and the amenities of the public frontages (street trees and plantings, benches, streetlights, etc.).





DEFINITIONS (*continued*)

**Streetscreen:** Sometimes called Streetwall. A freestanding wall built along the frontage line, or coplanar with the facade, often for the purpose of masking a parking lot from the thoroughfare. Streetscreens [should] be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building facade. The streetscreen may be a hedge or fence. Streetscreens shall have openings no larger than is necessary to allow automobile and pedestrian access. In addition, all streetscreens over four (4) feet high should be thirty (30%) percent permeable or articulated to avoid blank walls.

**Townhouse:** Syn. Rowhouse. (See Rearyard Building.)

**Transect:** a system of ordering human habitats in a range from the most natural to the most urban. This code is adapted from the SmartCode which is based upon six (6) Transect Zones which describe the physical character of place at any scale, according to the density and intensity of land use and urbanism.

**Transition Line:** A horizontal line spanning the full width of a facade, expressed by a material change or by a continuous horizontal articulation such as a cornice or a balcony.

**Transit-oriented development (TOD):** Development that combines a pedestrian-friendly environment with retail or entertainment ground floor uses, and located within one-half mile side-walk route to a premium transit station. Specifically, a TOD provides continuous, shaded sidewalks with street furniture and minimal driveway interruptions, and nearly continuous shop frontage.

**Transit station:** A transit stop incorporated into a principal building, and providing an air-conditioned area with television and electronic bus location information.

**Type:** a category determined by function, disposition, and configuration, including size or extent. There are building types, road types, open space types, etc. (See also: Building Type.)



DEFINITIONS (*continued*)

**Urban Greenway:** a multipurpose path accommodating bicycle and pedestrian traffic along major arterial roadways providing for a minimum clear zone from obstructions. (see Urban Greenways Illustration).

**Urban Grid or modified grid:** maximum block face of seven hundred (700) feet and maximum perimeter of two thousand (2,000) linear feet (approximately 3 acres). A maximum block face of eight hundred (800) feet and a maximum perimeter of up to three thousand (3,000) allowed for blocks with parking structures. Rear alleys or mid-block alleys required where feasible. The average block face in the TOC district is five hundred (500) feet.

**Warrant:** a ruling that would permit a practice that is not consistent with a specific provision of this Code, but is justified by its Intent. Warrants are usually granted administratively by the DRC.

